



2005 CITIZENS MASTER PLAN *for* D'IBERVILLE, MISSISSIPPI

MISSISSIPPI GOVERNOR'S COMMISSION

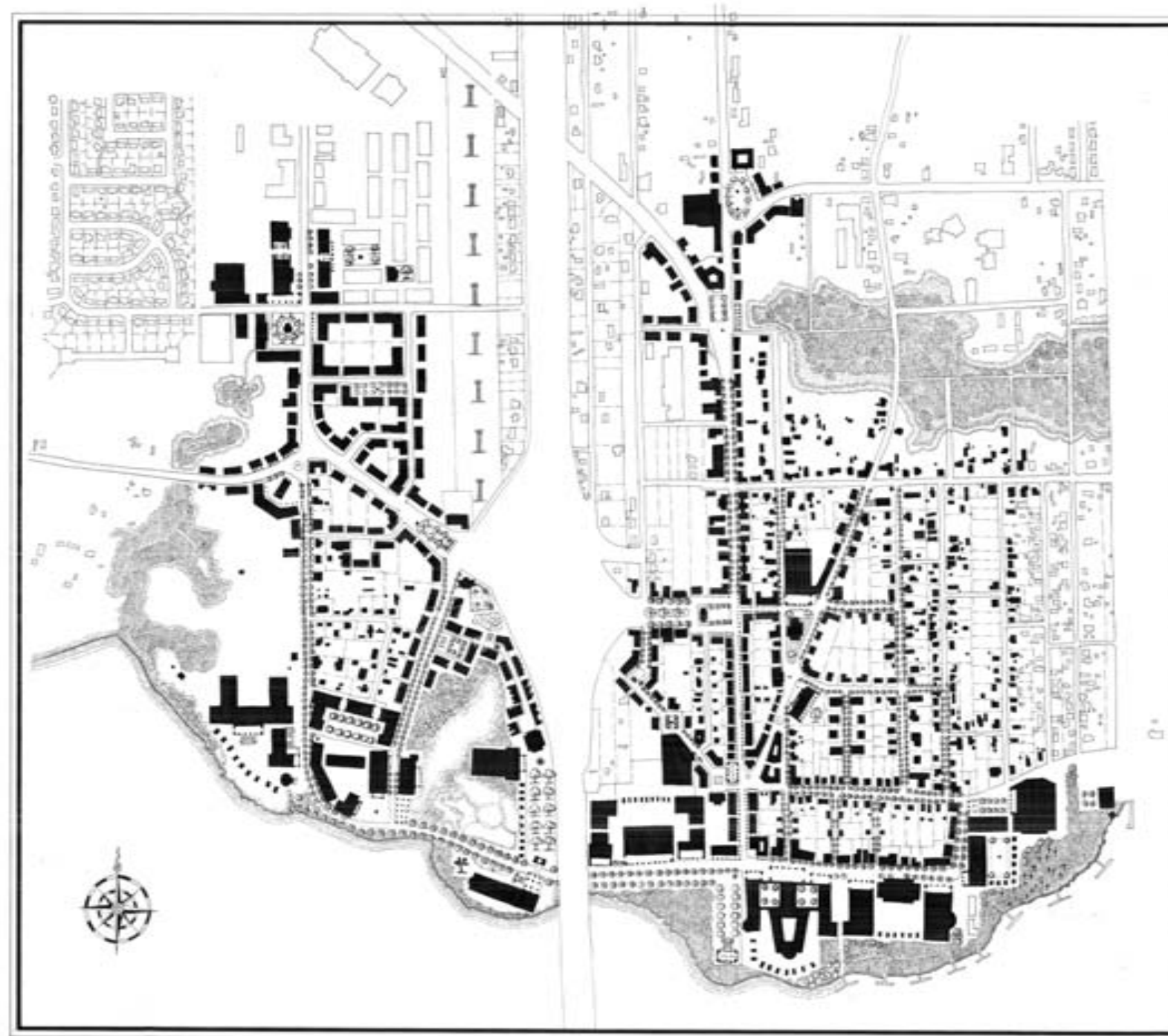




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Introduction



A.

THE 2005 D'IBERVILLE CITIZENS MASTER PLAN

Hurricane Katrina landed on the Mississippi area on Monday, August 22nd, 2005. In just a few hours, this powerful storm shattered the City of D'Iberville's visual identity, its sense of security, and its peace of mind. The devastation included some of its most beloved landscape, city spaces, waterfront areas, and buildings. For the citizens of D'Iberville, the tragedy might have removed some of its most important physical landmarks but, it did not erase its "hometown" feeling, its commercial success, or its community harmony—three characteristics which have turned the City of D'Iberville into one of the most desirable waterfront towns in the Mississippi Gulf Coast area.

With a communal sense of spirited determination, the Congress for the New Urbanism and the Mississippi Governor's Commission call into action a group of town designers, architects, engineers, environmentalists, planners, local merchants, business people, homeowners, investors, local officials, and



residents with one simple mission: to create a master plan for rebuilding and getting back what was lost at a regional and at a local level.

Andres M. Duany and the Congress for the New Urbanism (CNU), with the help of the Governor's Commission, and the collaboration of Jaime Correa and Associates, a Miami-based town and architectural design firm, came to the powerful realization that a tragedy of these proportions could only mean great and unprecedented development opportunities for their beloved environment. Just about one month had passed and yet the City of D'Iberville was being inspected by members of a design team charged with the task of creating a Citizens Master Plan for the reconstitution of the D'Iberville – a small region devastated by one of the most violent natural disasters in the history of America. As a result, the design team began a community process to facilitate the creation of a master plan, to guide future development, and to generate the greatest amount of social and physical benefits for the entire Mississippi Gulf Coast region.

This report represents the culmination of these efforts. It accommodates the goals and objectives of citizens, stakeholders, and public officials and also serves as a summary of the results of meetings with community leaders, a public presentation on October 17, 2005, and a design charrette in the grounds of the Isles of Capri Hotel and Casino in Biloxi, Mississippi.

While there is more community process to come, the discussion must now shift from planning to implementation of specific projects. At this point, it should be clarified that a master plan is purely illustrative and its compliance is voluntary. In effect, its illustrations are formulated as simple guidelines to help public officials, stakeholders, and citizens determine the overall value of any future proposals. The citizens must understand that without an enforceable set of instructions, the implementation of the community desires is NOT guaranteed.

Therefore, a new set of Land Development Regulations (LDR's) is imperative for the implementation of this master plan. The citi-

zens of D'Iberville must acknowledge the tremendous effort made by its public officials up to this day but, they should also realize that, the production of codes and/or regulations without a viable master plan is abstract and self-defeating.

The City staff and its public officials should take this opportunity to gain citizens support for the implementation of new regulating plans, urban standards, thoroughfare specifications, architectural guidelines, and landscape regulations. For instance, a new zoning map with the location of opens spaces and the type and trajectory of the various thoroughfares is of the essence; as it is the revision of those aspects of buildings which affect the public realm including, but not limited to, building setbacks, frontages, building height, encroachments, parking requirements, etc.; a matrix specifying dimensions for vehicular and pedestrian activities shall be assigned to the regulating plan; the permitted configurations and materials of walls, roofs, and openings must be delineated in order to avoid the lack of harmony characterizing suburban environments; and, finally, a list of plant spe-

cies (with instructions regarding their location and planting pattern) must be correlated to various locations in the urban to rural gradient of the region.

The truthful and uncompromised implementation of this plan shall allow you to reconstitute, not only what was taken by Hurricane Katrina but, the "Institutional Memory" of its citizenry, and the hope for a better and most productive future.

It would be impossible to arrive at this day without the dedication and commitment of many individuals and organizations. We at JAIME CORREA AND ASSOCIATES look forward to continuing our partnerships as we share with you the scope, joy, and hope of this historic job.

Brief History



A. Pierre le Moyne d'Iberville

In February, 1699, Pierre LeMoyne Sieur d'Iberville anchored off the Mississippi Coast in three large ships, with 200 French, Canadian, and West Indies settlers and a contingent of 50 French marines, at what they called "Ship Island."

A Canadian naval hero, Iberville had been charged by France's King Louis XIV with completing the failed final mission of the explorer LaSalle to find the Gulf outlet of the Mississippi River and to establish French footholds-colonial settlements and outposts-throughout the lower Mississippi Valley.

Tides and weather permitting, Iberville took a small landing party onto the coastal mainland and made fast friends with the natives.

They assisted him in finding the mouth of the river. This first task completed, he built a small fortress, which he named "Maurepas" for the family name of Count Ponchartrian, the French Minister of Marine. At the fort, he then laid plans for settlement and ultimate French colonization of the region as part of "New France." He named the vast areas he sought to colonize "Louisiana" after the "Sun King" Louis.

While it is now generally agreed that Ft. Maurepas was located in nearby Ocean Springs, scholars still dispute where the original French settlements named "Biloxy" after the local indian tribe, was located in Biloxi, Ocean Springs or D'Iberville. Regardless,

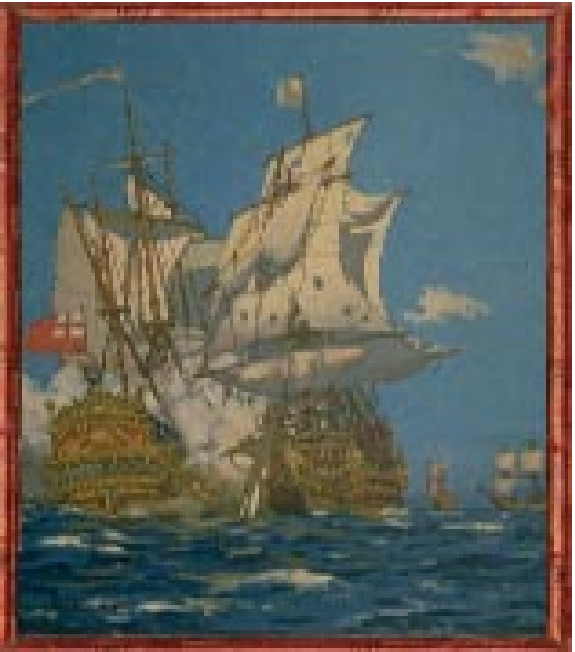


B.

anthropological evidence indicates that some form of settlement or outpost was established to some extent in all three areas. “Old Biloxey” served as the first capital and base of colonization of the entire Gulf South, including the subsequent establishment of settlements which became Mobile, New Orleans, Baton Rouge, Jackson, Natchez, Montgomery, and what is now much of the states of Louisiana, Mississippi and Alabama.

While the legendary Iberville was generally credited as a founder and thus, is the namesake of the community, his brother Sauvoie and Bienville and their french followers were unable to sustain a continuous settlement here. It was the Spanish who had the most influence on D’Iberville’s early development

and permanence as a settlement. French colonists, used to a more civilized environment, found the harsh wilderness here difficult to endure. By the mid 18th century, with



C.

A. Map seal
showing arrival
to the City of
D'Iberville.

B. Inaugural mo-
ment.



A.

disease, pestilence, and skirmishes with the English, Spanish, and hostile Indians, few French families remained.

As a result of its losses to the English in the French and Indian wars, France ceded most of its North American holdings to England, with one notable exception-The Louisiana parishes east of the Mississippi, the coastal regions of Mississippi, Alabama, and the Florida panhandle were given to Spain.

Collectively known as West Florida, the territory was ruled by a Spanish governor in Pensacola. By 1770, only five of the original French families had remained in the present day Biloxi-D'Iberville-Ocean Springs area. Some of these relocated to the now larger communities of Mobile or New Orleans. Concerned about their holdings in the central

part of West Florida, Spanish authorities sent a small contingent here to occupy and govern the area. Led by a garrison soldier named Juan de la Cuevas, the area began to flourish, attracting quite a few Spanish families over the next several decades. This Spanish occupation explains the preponderance of Spanish surnames found among some of the oldest surviving families in D'Iberville: Diaz, Rodriguez, Gutierrez, and Santa Cruz.

By the early 1800's, Spain had lost most of its claims east of the Mississippi, but its influence continued. Many Americans moved into the Gulf Coast territory. Around 1840, Biloxi and Pass Christian, and other coastal towns were touting themselves as a welcome refuge from the disease and urban ills of the



B.

Story submitted by Boy Scout Troop 212

- C. *Foundation of the City of D'Iberville.*
- D. *Cemetery.*
- E. *Foundational stone.*
- F. *Interstate highway 110 to Biloxi.*

larger cities. The D'Iberville enclave of small communities, with vast natural resources, saw opportunities for providing the materials needed to support the growth of these resorts. Timber and its by-products, vegetable farms, fruit and nut orchards, sawmills, brickyards, and boat builders brought prosperity to the area. By 1860, the D'Iberville area had a population of around 500.

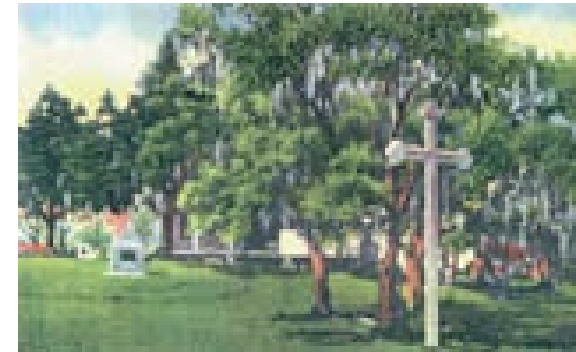
Progress was cut short by the War Between the States when most men went off to war. The entire Gulf Coast was suppressed for two decades until conditions improved in the 1880's. By the turn of the century, D'Iberville had grown into a town of over 1,000 people. The town's economic and agricultural base continued to sustain the community even

through World War I and the Great Depression. Seafood, lumber, and produce continued to be the town's mainstay.

World War II brought the construction of Keesler Air Force Base. With base expansion, D'Iberville experienced tremendous growth with new subdivisions and shopping centers well into the 1970's. In spite of it all, D'Iberville has maintained its own independent pioneer spirit, viewing its community as having its own strong identity. Consequently, in 1988, D'Iberville officially became a city—an incorporated municipality of the State of Mississippi. It is fitting that this momentous occasion occurred on February 13th, almost three centuries to the day that Iberville landed in the area.



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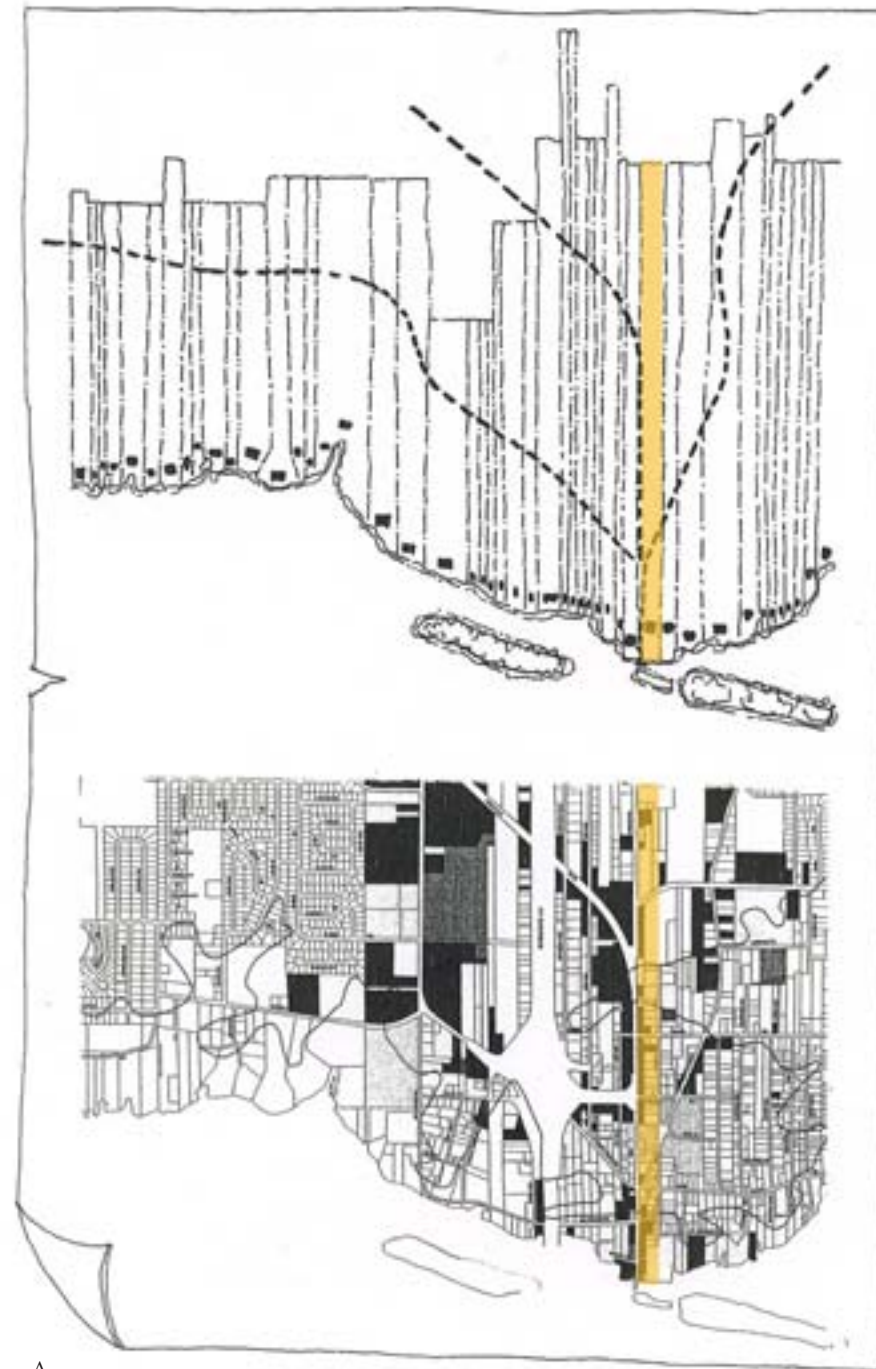


E.



F.

Urban Form Characteristics



The traditional layout of the city reveals important clues about its development history. For instance, the land subdivision inherited by the French System of Surveying land (based on units of measurement known as “Arpents”) shows how the entire territory was meant to be a conglomeration of waterfront houses on lots of various widths along pre-existing rural roads. Throughout the years, and adding to the variety offered by the French system, these elongated lots were further subdivided into north-south increments of variable dimensions (most likely based on a unit of measurement known as “Chains”). The consequent result was a territorial subdivision of elongated blocks containing a variety of lots and street widths.

The traditional layout established the general order of things. Three pre-existing regional roads converged at Central Avenue where the most important connection to the main island of the City of Biloxi was established –by means of a simple boat link first, and a bridge later.

A clear distinction between civic and domestic buildings is evident from the very beginning; as it is the configuration of commercial pathways. But, most importantly, the development of the existing urban form allows the normal resident of D’Iberville to understand the spatial hierarchy of its public spaces and the significance of specific buildings in the overall context of the city.

The typical houses had a simple configuration. Simple wooden structures with ample porches, small towers, gable roofs, and operable windows provided ventilation and cross-ventilation. The commercial buildings, on the other hand, were deep and narrow (40 ft. to 60 ft. by 18 ft to 24 ft.). Storage buildings and small shops occupied the most prominent sites along the riverfront. Civic buildings demonstrated their community presence by means of their construction materials and their iconographic character –brick and stone were the most common civic materials.

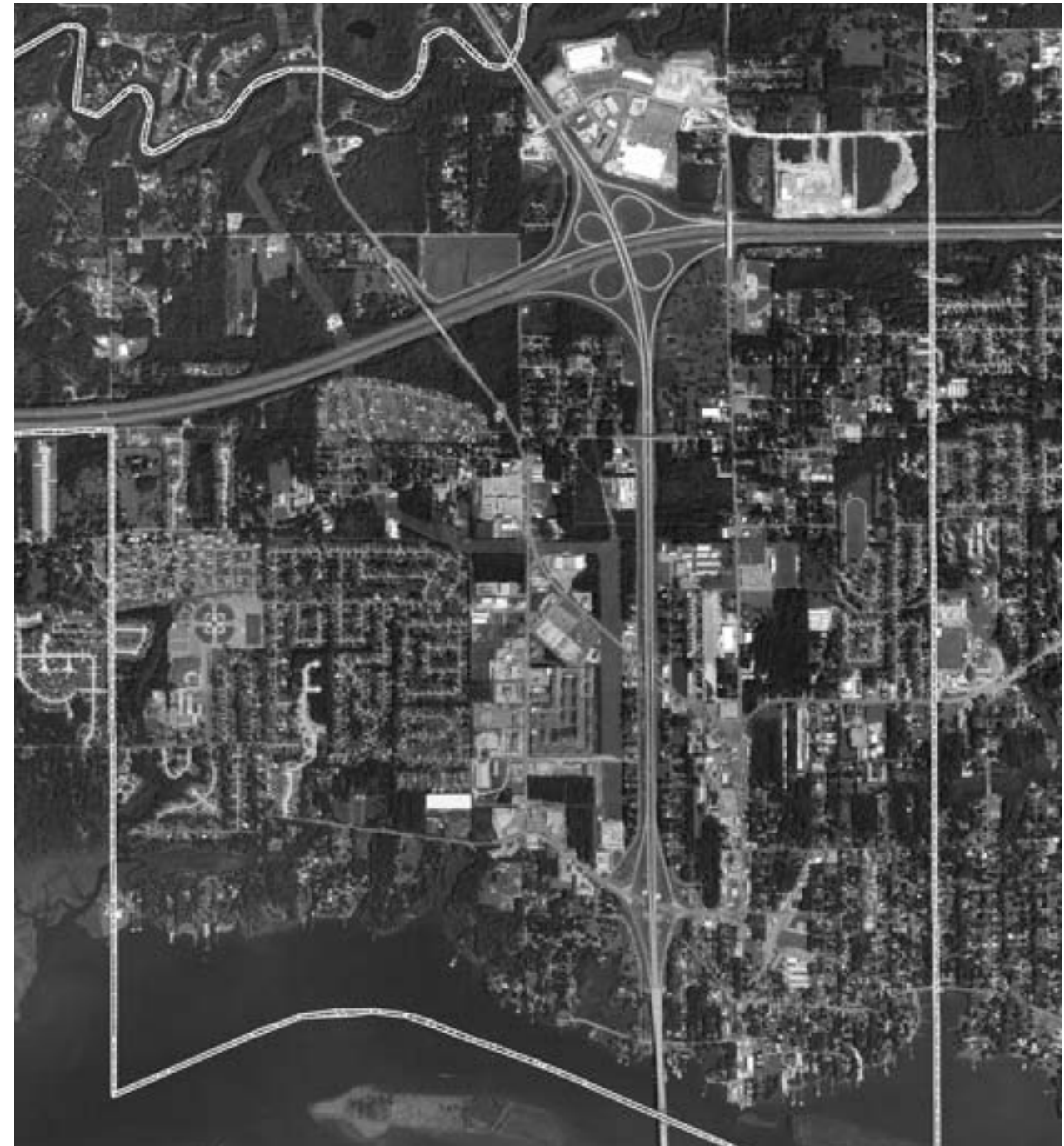
The city's landscape was extremely natural and vivacious. Its lack of coherence provided a relaxed character and a respect for existing Mississippi conditions. Beautiful tropical and sub-tropical domestic gardens were wild and untidy. Parks and waterfront areas were informal and conducive to a more rural community building.

As the town continued to grow, several events determined the future of the city as we know it. First, the incorporation of the City in 1988 allowed its public officials to control its residential and commercial growth; new land development regulations and zoning ordinances have eradicated the traditional system of neighborhoods, blocks, and streets, and have advocated the creation of walled communities; The arrival of Wal-Mart and Lowe's improved the City's tax base by almost 30%; the annexation of unincorporated territory has extended the City beyond its original limits –and, there are still plans to annex other portions of the adjacent suburban territories; and finally,

the changing face of the city was completed upon the construction of the two dividing highways I-110 and I-10 -and its elevated bridge to the City of Biloxi.

It is important to clarify that cities do not get built in one day. The notion of a “collective memory” worth of reconstitution is pervasive amongst the citizens of D'Iberville. Although it has never been articulated, they yearn for a small town feeling where the architecture of the city enhances the idea of community and where everyone feels in control of their own destiny.

Hurricane Katrina has provided the City of D'Iberville with the extraordinary opportunity to reconstitute its collective memory. In addition to being the representation of identity and cooperative wishes, the 2005 Citizens Master Plan is a testimony to community excellence and, as of today, the clearest blueprint for a better future.



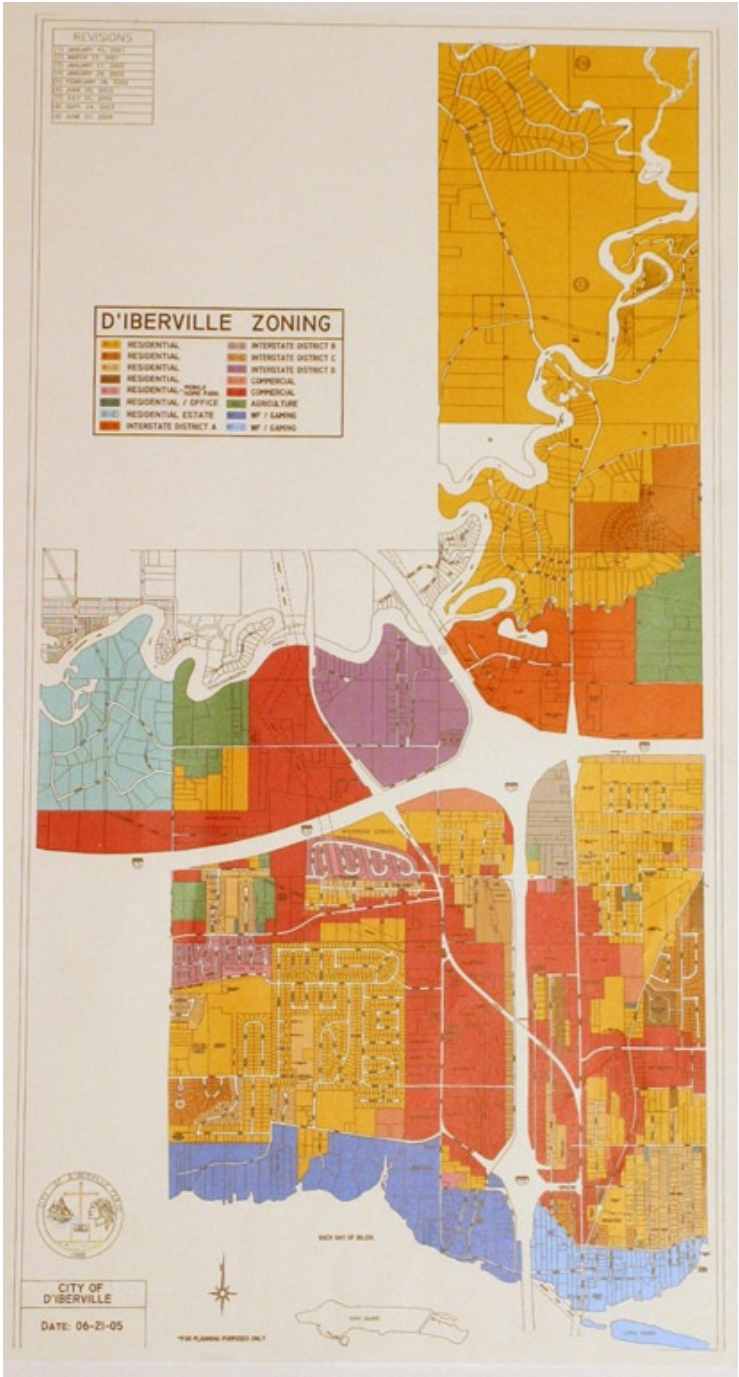
A. Land Use Map.

B. Francisco Llado and Grace Dillon discuss the plan during the charrette.

C. Design team and public officials during charrette tour. From right to left: Manue de Lemos, Grace Dillon, Jaime Correa, Mayor Quave, Bobby Eleuterius (County Supervisor), Richard Rose (City Manager) and Mark Seymour (Engineer).

D. Charrette Room in Biloxi.

Methodology



A.

This project was designed by a multi-disciplinary group of professionals with one single mission: to translate the development desires of a community badly hit by Hurricane Katrina into projects with implementation potential. Andres Duany, with the help of the Congress for the New Urbanism and the Governor's Commission, provided a design team composed of more than 130 people including: urban designers, retail specialists, traffic engineers, housing specialists, landscape designers, environmentalists, code



B.



D.

specialists, redevelopment experts, public relations specialists, and logistics experts.

Jaime Correa and Associates, a design team with 4 town designers and architects, collected documentation from institutional, phenomenological, existential, and pragmatic sources. Amongst the institutional sources of information, the following may be the most important: the City of D'Iberville and Harrison County Comprehensive Plans, the City of D'Iberville Land Development Regula-



C.

- E. *Jaime Correa and Mayor Rusty Quave discuss the devastation during the charrette.*
- F. *Proposed redevelopment plan by others.*
- G. *Proposed FEMA deileanations.*
- H. *Proposed handling of new FEMA flood requirements.*



E.

tions, the 2004 Demographic Profile, reports generated during previous planning processes, publications on the history of D’Iberville, existing and future Land Use Maps, and many other sources of documentation and research provided by local officials and concerned citizens.

Phenomenological sources of information were collected during one-to-one interviews with public officials, local historians, and old timers. This important information sup-

plied the design team with a complete historic recollection of the City of D’Iberville at its heyday and with community goals and objectives.

Existential information was collected during public workshops and presentations. This information was mainly received from a number of stakeholders, and public officials who, by their own volition, decided to participate in the workshops and presentations lead by members of the design team. The input re-

ceived during these events acquired the highest degree of priority, helped the design team to establish value judgments, disclosed the real costs and benefits of living in a small Mississippi town, and defined the most achievable and realistic development goals.

Finally, during round-table meetings with public officials and FEMA representatives, a multiplicity of other pragmatic issues were discussed. The concerns of these groups of interest were taken into consideration but,

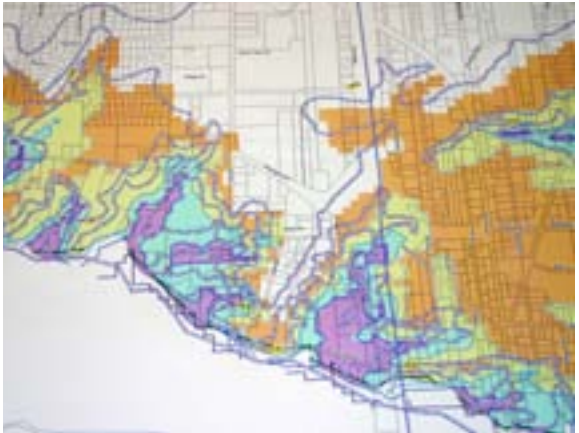
they were never included at the expense of the desires of the community. Many of the on-going development proposals ended up with design configurations which made sense in the general scheme of the master plan and which complied with the programmatic and financial requirements of its generators.

The issue of the new Base Flood Elevation provided by FEMA does not interfere with the goals and objectives of this master plan. It is important to take into consideration their recommendations; nevertheless, as presented by FEMA, the coastal areas are the only ones clearly affected by their decisions; the master plan and the public officials of the City of D’Iberville, however, have particular plans to dealing with these issues.

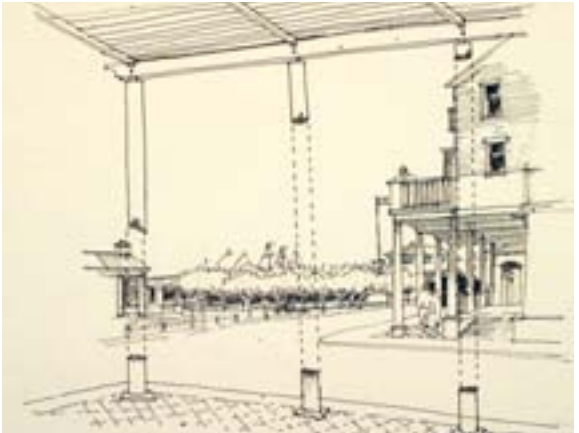
The charrette work was carried out between October 11th and October 17th of 2005 at the Isles of Capri Hotel and Casino in Biloxi, Mississippi.



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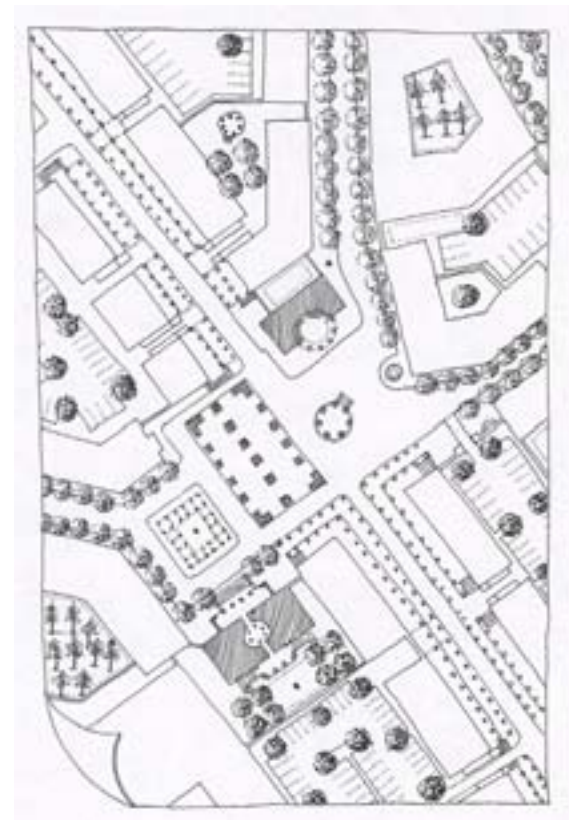
The Master Plan



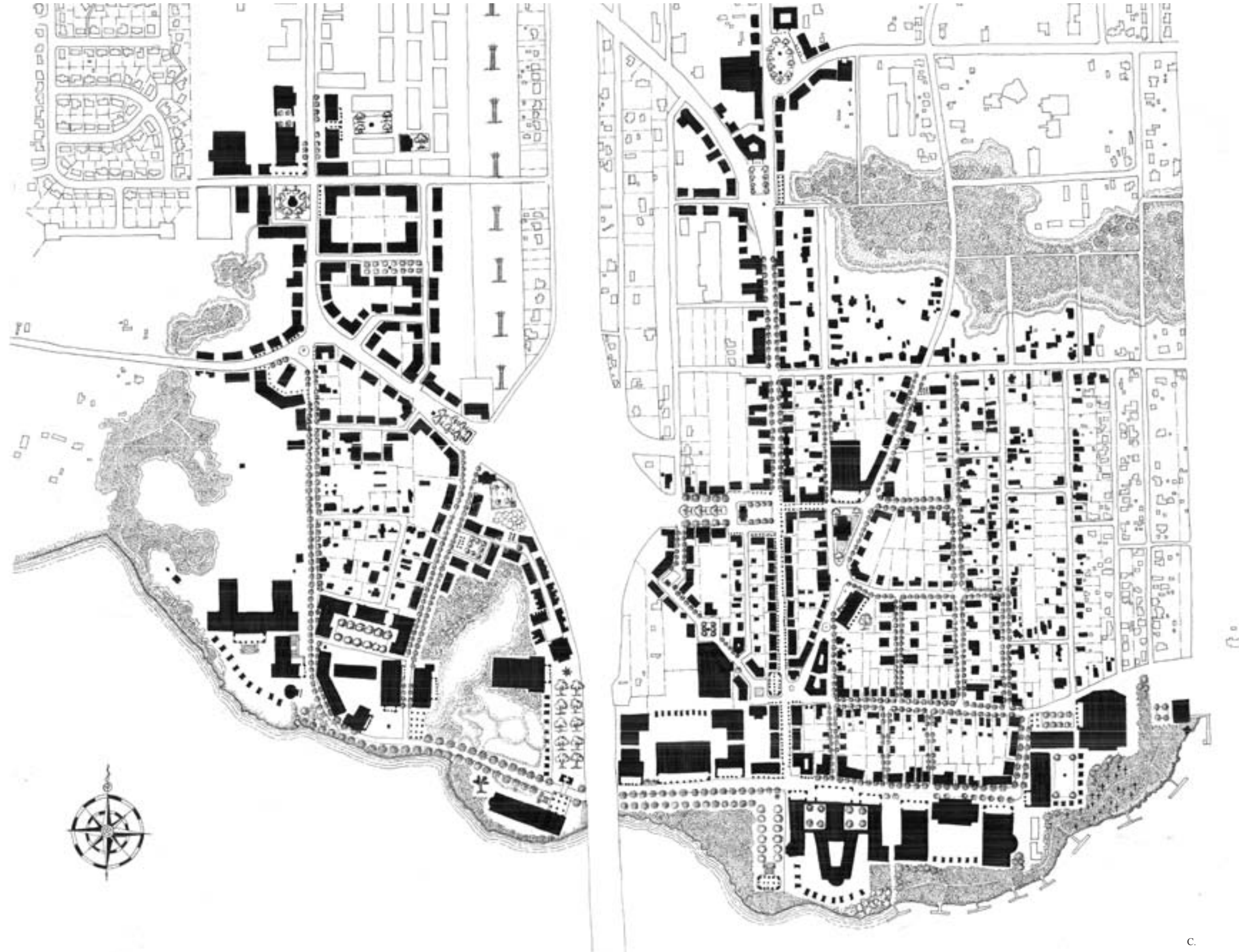
A.

The D'Iberville 2005 Citizens Master Plan has four parallel components: First, the LOCATION MAP, understanding the city in relationship to the entire Gulf Coast region; second, the REGIONAL MASTER PLAN establishing the organization of the D'Iberville region including the definition of urbanized areas, potential annexations, environmental protection, open space, and quality of life; then, the DETAILED MASTER PLAN

focusing on the scale of the neighborhood, the block, and the public space –including the street, the sidewalk, the waterfront, and the various urban space categories; and, finally, the SPECIAL URBAN DETAILS sections depicting proposals for a collection of individual parcels, the architectural syntax and character of future proposals, and other guidelines and landscape standards.



B.



C. Detailed master plan.

A. Location of
Route I-15 (un-
der construc-
tion.)

B. Location map.

Location Maps



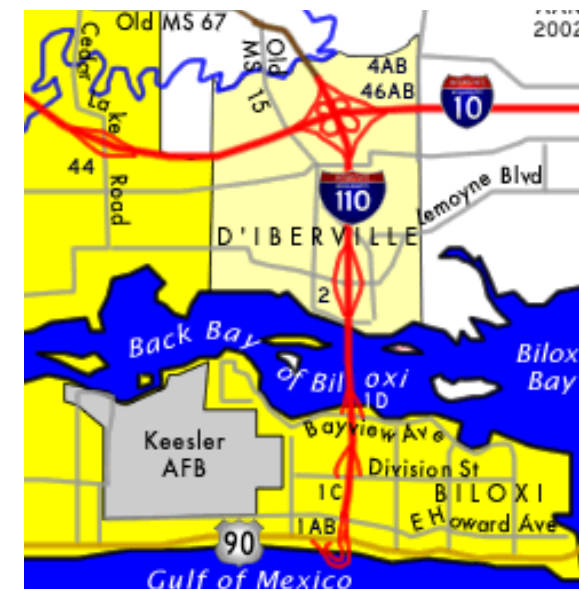
A.

The City of D'Iberville is located north of the City of Biloxi on the inner bay. At its current location, it is severed into four quadrants by I-110 and I-10. The City also has annexation plans; as established in its comprehensive and future land use plans, it is currently studying the possibility of annexing areas to its north-west side. Before the hurricane, the City of D'Iberville had approximately 7500 permanent residents and occupied approximately 5.4 square miles of suburban/rural territory.

I-15 a Shortcut of Opportunity:

For a long time, the intersection of Route 49 and I-110 had been the commercial center of the Mississippi Gulf Area. As a commercial node, this center was fed by a continuous north-south and east-west traffic to and from the City of Biloxi. Located within the City of Gulfport, this area achieved tremendous gains for this region and still continues its growth at an accelerated pace.

Nevertheless, the rules changed when the Department of Transportation decided to build a new shortcut from Route 49 to I-110. The new diagonal shortcut (I-15) will save between 25 and 35 minutes of travel time to north-south travelers and will create a more direct connection to the City of Biloxi. As a consequence, the intersection between I-110, I-15, and I-10 will generate the greatest amount of traffic counts and will eventually re-define the regional center as the most important commercial node in the region. The City of D-Iberville must not be oblivious to this opportunity.



B.

A New Loop Road:

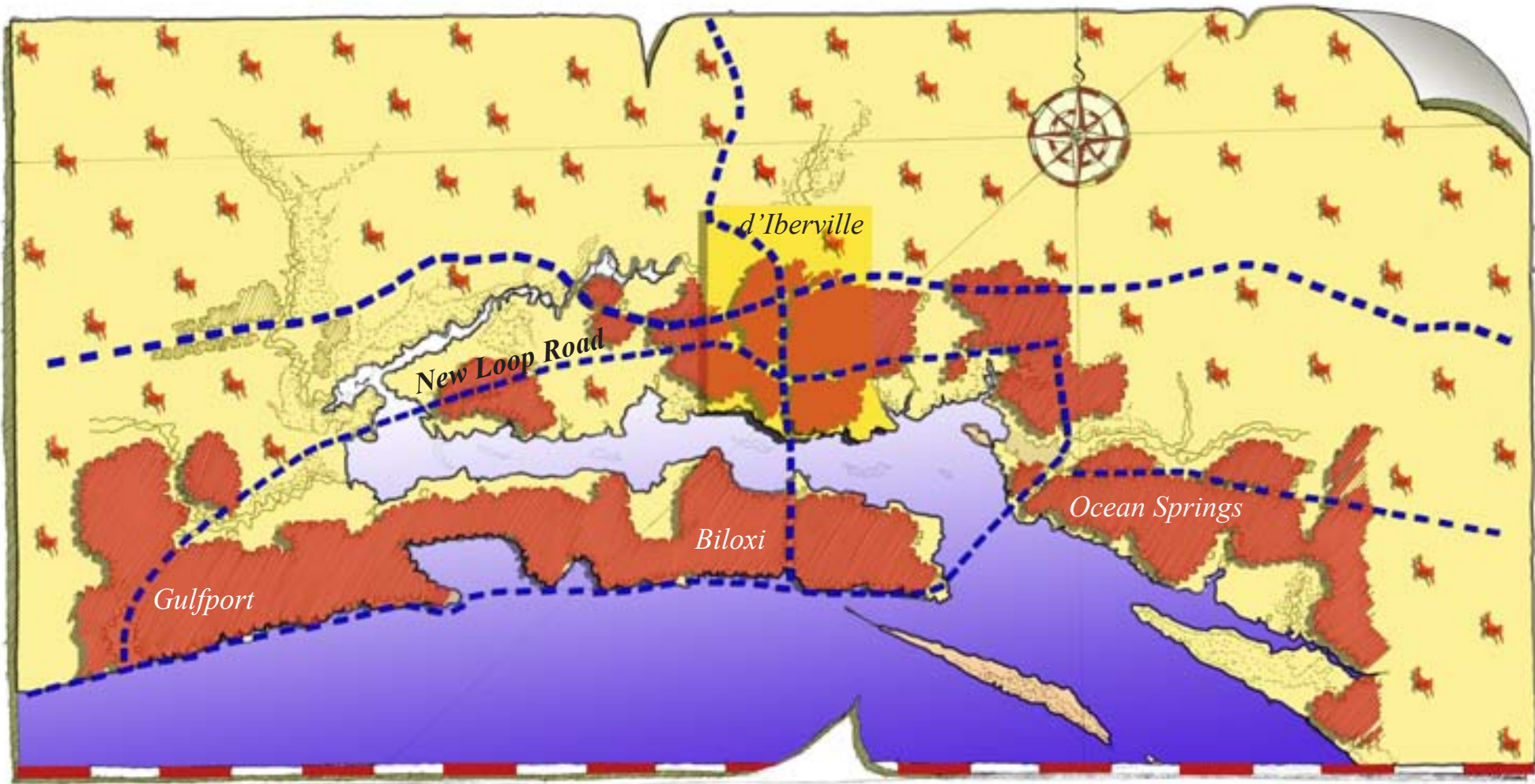
After Hurricane Katrina destroyed the bridge between the City of Biloxi and the City of Ocean Springs a new connecting route became essential for the temporary traffic health of the Gulf region. In an attempt to improve

the local traffic conditions within these areas, the City of D'Iberville public officials identified a potential connection between the City of Gulfport and the City of Ocean Springs. To achieve this goal, the master plan propos-

es a simple on grade re-connection between Popps Ferry Road and Lemoyne Boulevard; this intervention would create this much needed traffic loop.



C.

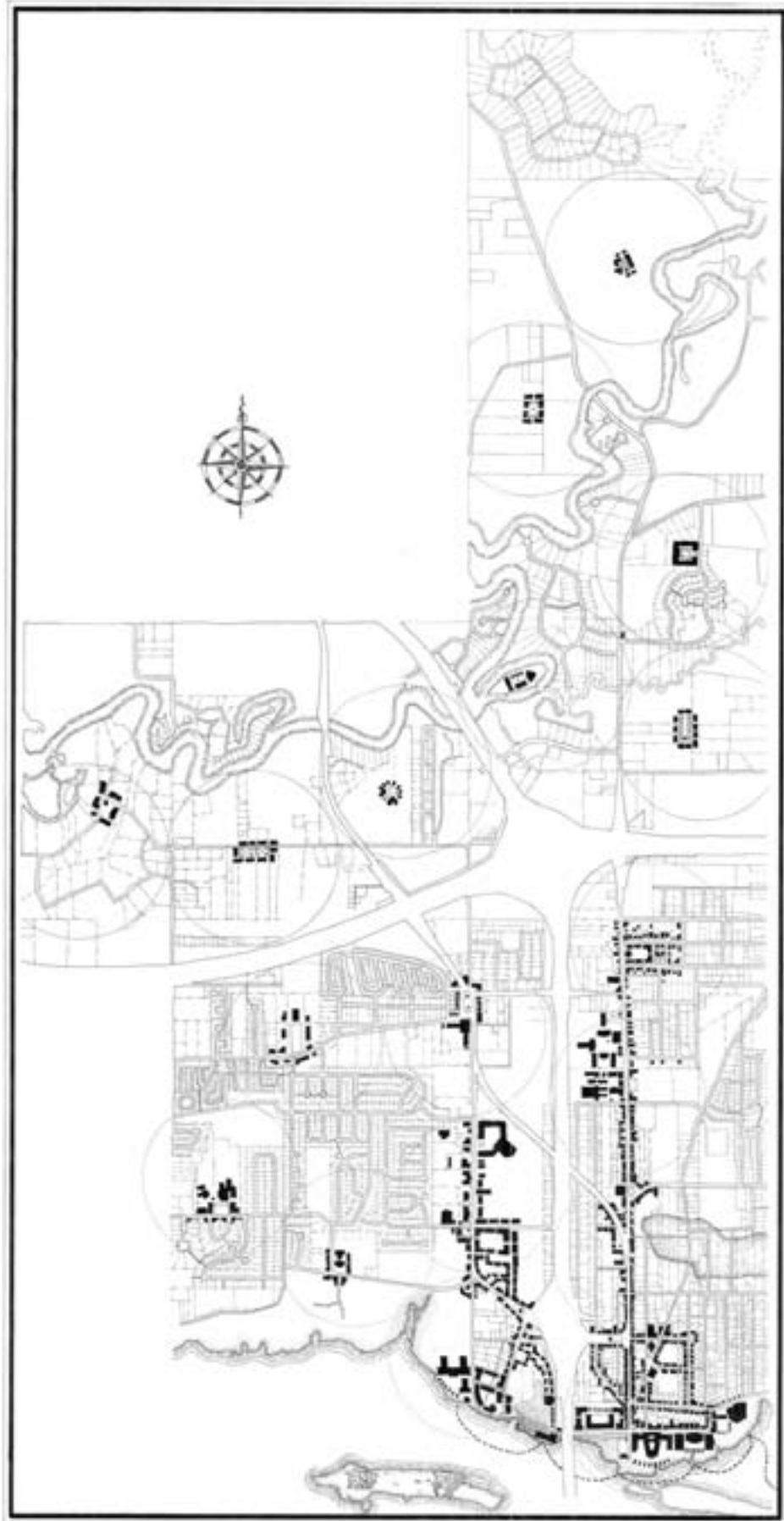


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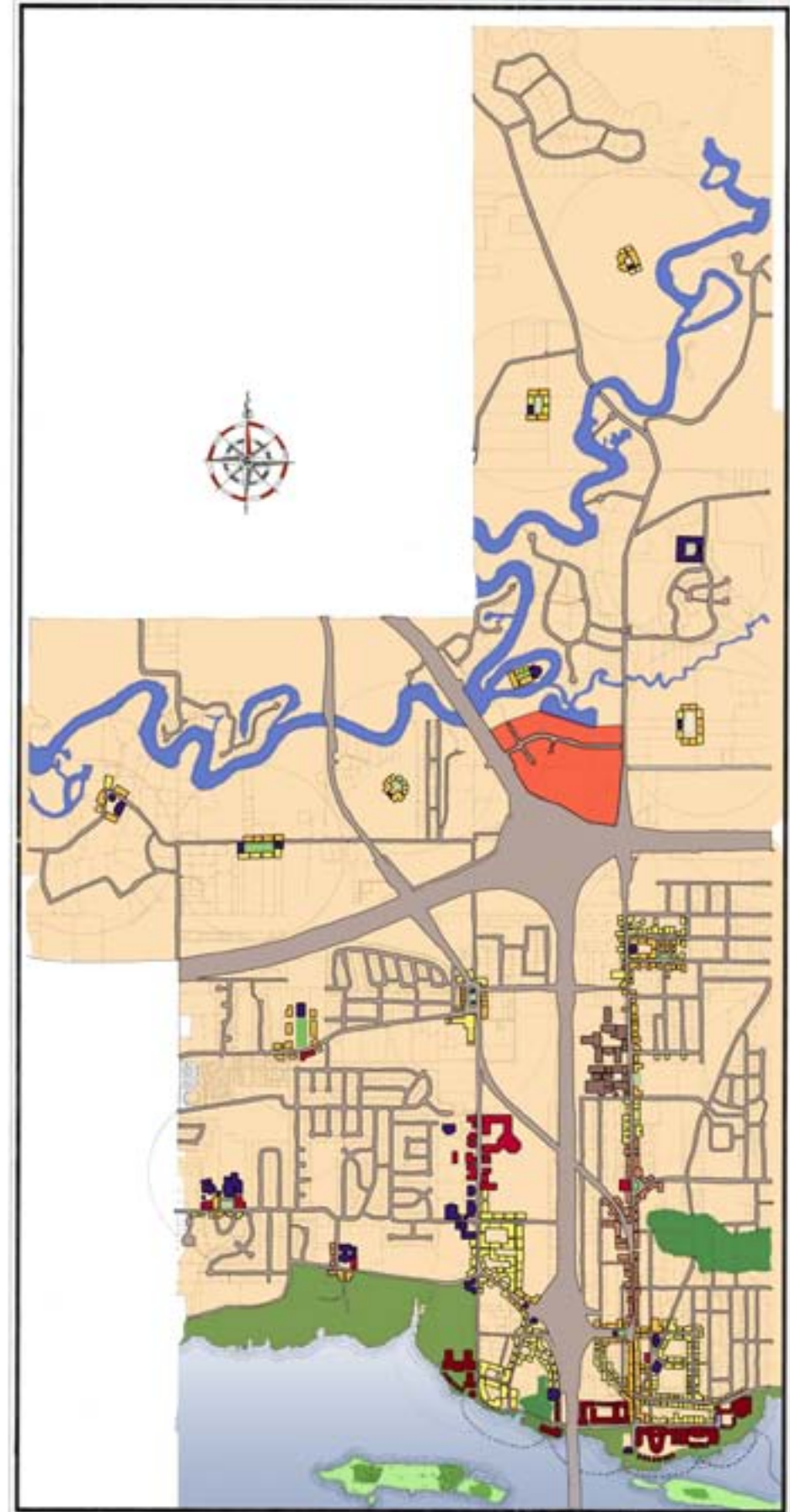
R e g i o n a l P l a n

The regional master plan establishes the organization of the D'Iberville region. It includes a hierarchy of its urbanized areas, the location of its supporting districts, the potential for the annexation of neighboring territories, the location of open space corridors and areas of environmental protection,

and particular strategies for the development of waterfront areas and regional landscapes. Its main goal is the mapping of a predictable and a defensible future where the traffic congestion, the fragmentation, and the dissatisfaction of conventional suburban sprawl are NOT necessary the design criteria.



A.



B.

A. Regional Map depicting centers, neighborhoods and proposed buildings footprints.

B. Proposed land use regional master plan.



The Neighborhood Requirements:

The regional master plan sets apart specific areas of the D’Iberville territory for neighborhoods with various scales and character. The neighborhood is hereby understood as a comprehensive planning increment which varies in population and density to accommodate localized conditions. A neighborhood is limited in size so that a majority of its population is found within a 5-minute walking distance from its center (approximately ¼ mile).

In theory, the needs for daily living should be available within the neighborhood proper. In practice, due to commercial demands for servicing support, it is sometimes necessary to share these areas and locate them at the

articulation of two or more neighborhoods. The elderly and the young benefit, and gain independence of movement, by bringing most of the activities of daily living into a walking distance.

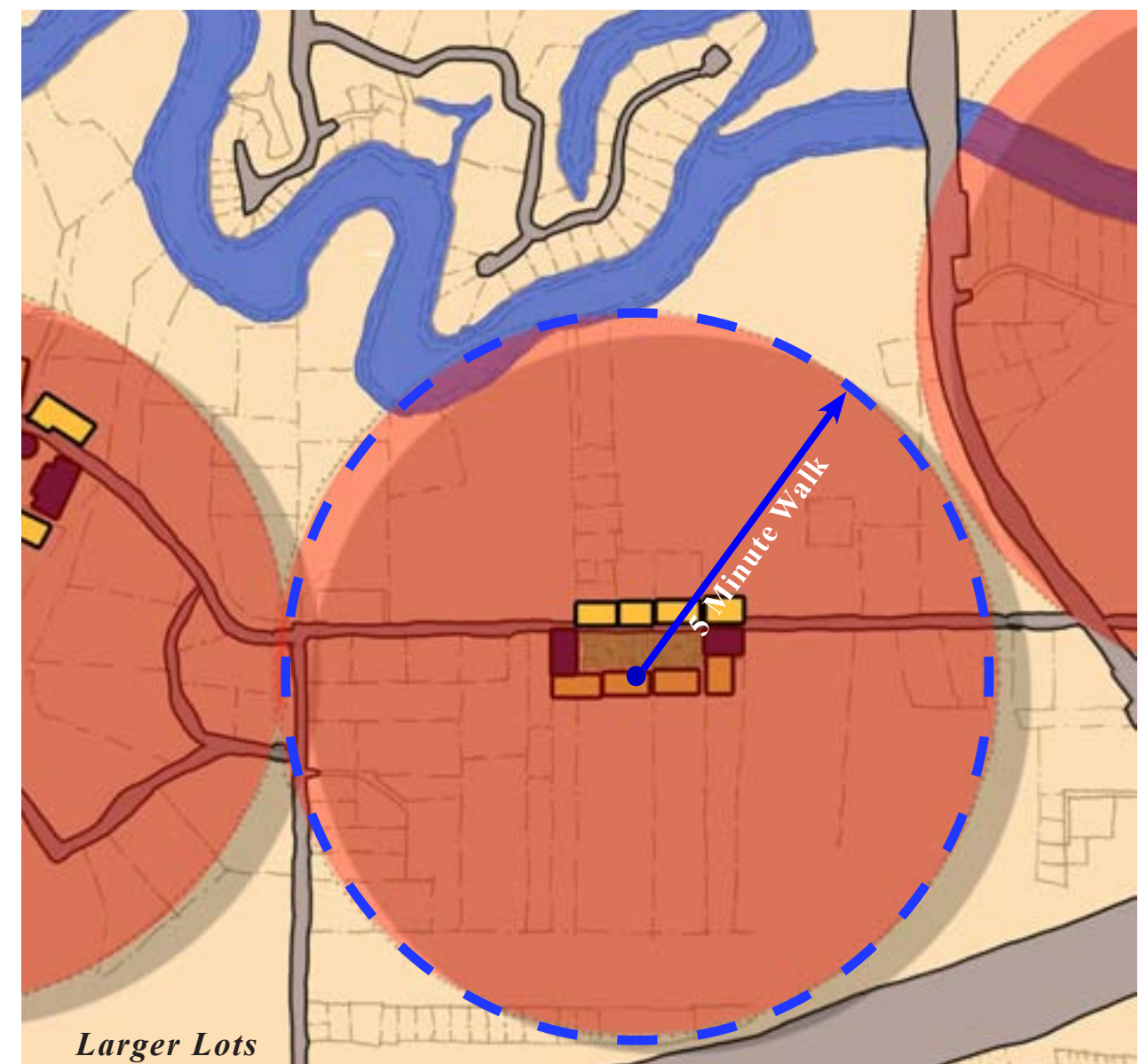
The neighborhood streets shall be laid out in a knitted network, so that there are multiple alternate routes to every potential destination. Therefore, the neighborhood streets can afford to be smaller, with slower traffic, on-street parking, trees, sidewalks, and building frontages defining urban/suburban public rooms. By reducing the number and length of automobile trips, the expenses of road construction and traffic congestion are minimized.

Neighborhoods shall contain one center and a variety of open spaces for the location of civic buildings (day-care centers, meeting halls, pool houses, condo-clubs, museums, churches, libraries, etc.). By providing streets and open spaces of comfortable scale, the residents can come to know each other, may get encouraged to develop democratic initiatives, and may watch over their collective security.

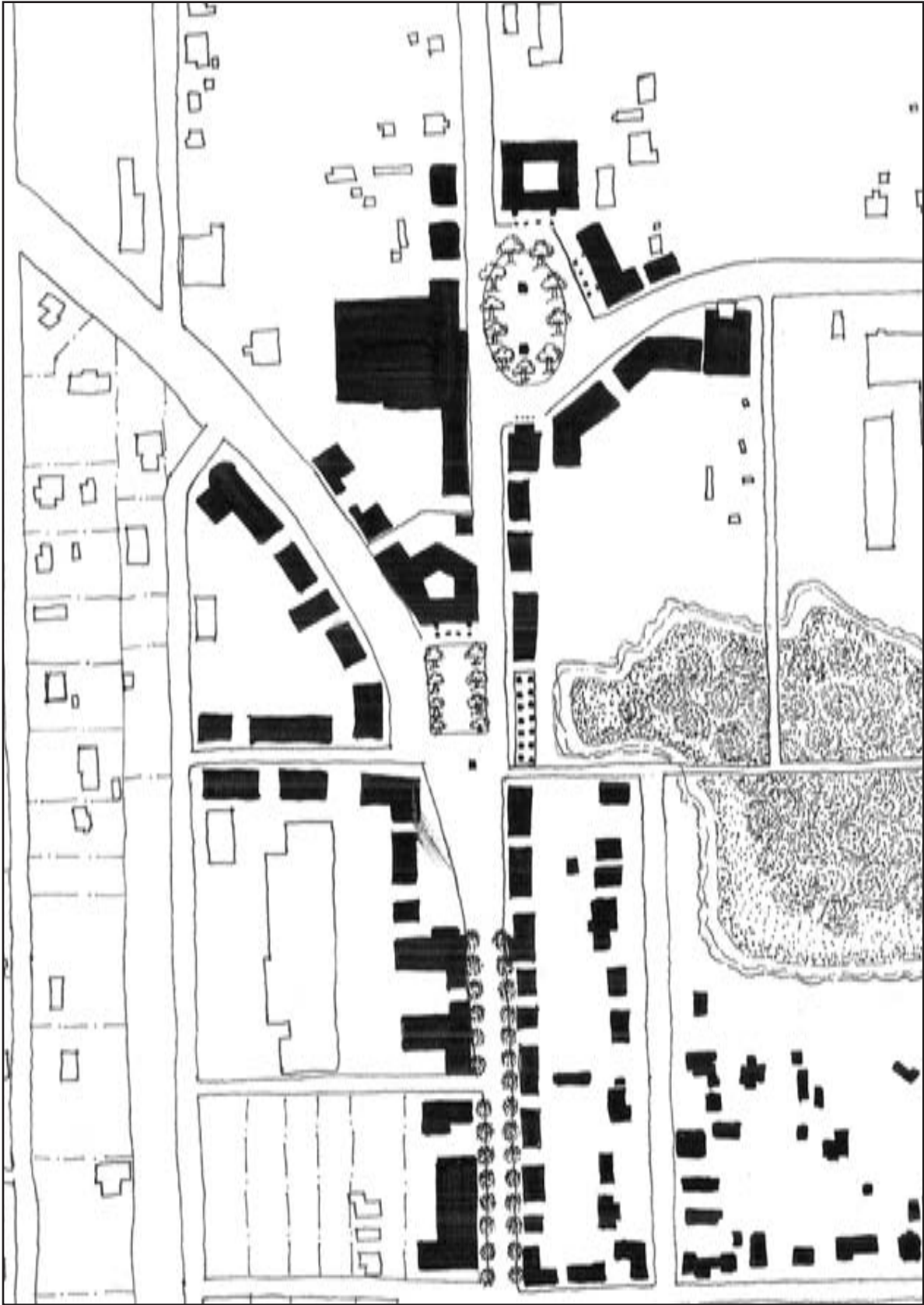
Neighborhoods provide a wide collection of building types. A full range of housing types, work places, age and income distributions, and economic classes should be integrated to form authentic communities. Nevertheless, there shall be a gradient of neighborhoods -going from the more urban to the more ru-

ral. The more central the neighborhood is, the greater its density should be. To achieve economies of scale and concentration of services, rural neighborhoods may afford an increase of up to 40% the size of a regular neighborhood.

A neighborhood is NOT composed of sprawling areas, pods, gated communities, office parks, residential clusters, large housing projects with segregated areas, units of similar cost, 50 MPH retail, cul-de-sacs or looping streets, parking lots dominating the public space, highly articulated buildings, buildings placed randomly in and out on the lots, civic buildings located on residual spaces, buffers, “berms”, or other ill-defined spaces or buildings.



A. Live/Work district detail.
B. Live/Work district location.



B.

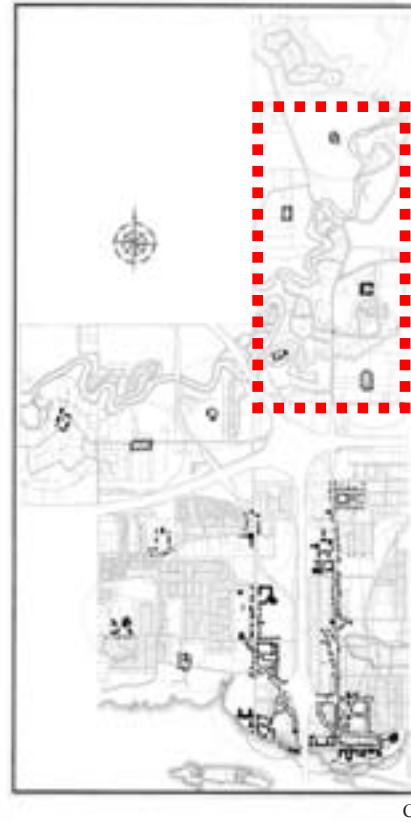
The District Requirements:

A district is a specialized section within the region. Districts are typically occupied by land uses which, by their function, are incompatible with other pedestrian oriented areas within the region. Nevertheless, the design of districts should be held up to the same standards of the neighborhood unit.

Whenever possible, a district should have a mix of land uses conducive to the creation of life on a twenty-four hour basis. To resolve the contradiction of land use incompatibility and pedestrian activities, districts shall use Live/Work units.

Districts are typically used to supply the demands of the new economy. Typical districts include areas with gigantic distribution and warehouse centers (storage areas for large corporate Big Boxes i.e.: Walmart, Winn-Dixie, Lowe's, K-Mart, Target, etc.), a collection of Big/Boxes (also known as a Power Centers), or for premises available for the creation, assemblage, and repair of artifacts including their retail sales i.e.: art districts, entertainment districts, storage districts, fashion districts, civic districts, etc.

The D'Iberville Region is composed of various neighborhoods and districts:



C.

C. Rural neighborhoods diagram.

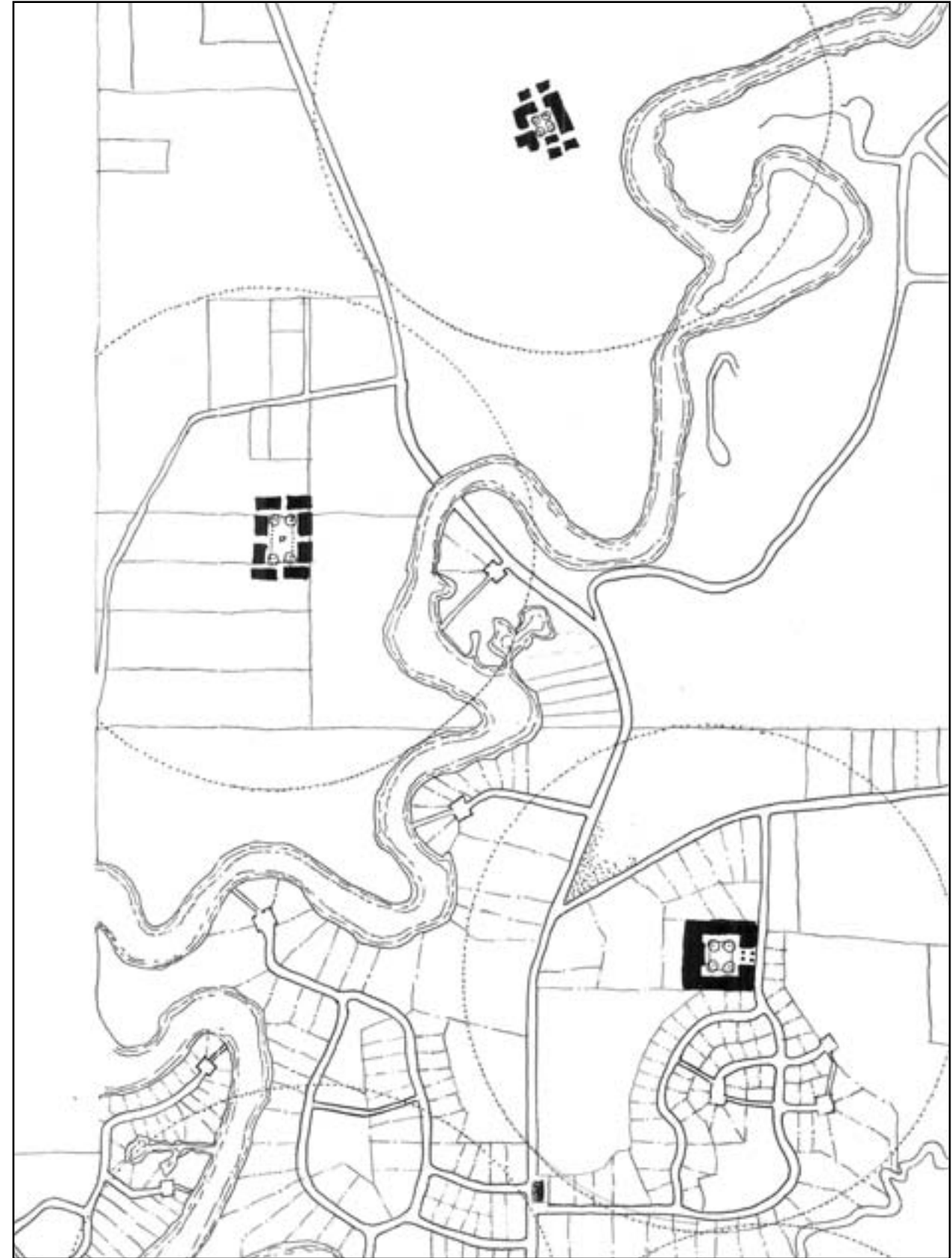
D. Rural neighborhoods location.

North Side Rural Neighborhoods:

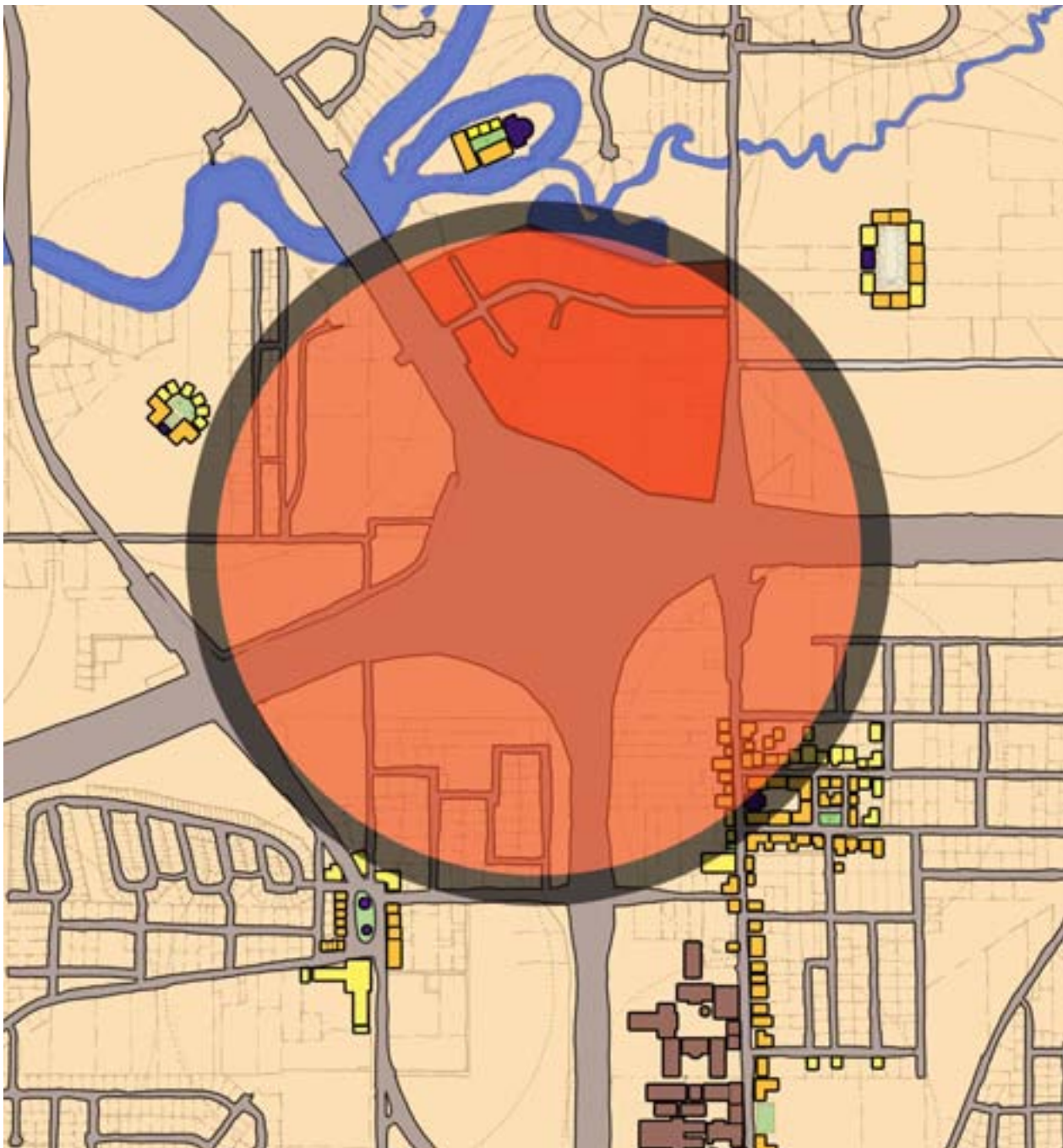
The D'Iberville regional master plan assumes a neighborhood composition with a rural to urban gradient. The most rural neighborhoods would be located in the north—with a variety of lots in excess of $\frac{1}{4}$ of an acre; the most urban neighborhoods, in the center, with a minimum lot width of approximately 18 ft.—a lot appropriate for a typical small live/work unit or a row house.

The rural neighborhoods are located north of I-110. The preservation of their rural character, by means of density controls, is important to the overall health of the region. These rural neighborhoods should occupy a territorial area not to exceed 40% the normal

area of a typical neighborhood. Club houses, common facilities, or central parks shall be located at their approximate geographic center. Daily living activity centers shall occur at the intersection of the neighborhoods; these facilities may include: convenience stores, convenience retail (barber shop, bakery, etc.) schools, clinics, medical offices, police or fire sub-stations, gyms, library branches, churches, social clubs, etc. As a rural area, the roads may be smaller and/or curvilinear, the buildings may be setback at irregular intervals, the landscape may be planted in a more informal manner, an abundance of picket fences should be highly encouraged, and sidewalks become an option.



D.



Wal-Mart Power Center District:

The undeniable intensity of this new district is completely overwhelming. Despite the presence of some of the most important “Category Killers” almost 30% of the current tax-base is generated by the commercial establishments within this district; its commercial value is irrefutable. Therefore, it is essential to recognize the strength of this new development trend and realizing that, it is better to conglomerate these land uses together than to let them grow without any control.

Another important development factor will occur with the current extension of Route 15 to Route 49. This new shortcut to the City of Biloxi will undermine the survival of regional malls in Gulfport (saving almost 25-35 min-

utes of travel time to North-South travelers) and will, definitely, create an important commercial hub at the corners of Inter-state 110 and Inter-state 10. There are growing pressures for large-scale retail development at the intersection of the highways. Residential growth, demographic trends, higher income households, tourism, and growth pressures - in other parts of the Gulf Coast, will turn the D’Iberville region into an attractive location for big box retail development and shopping centers.

The proposed regional plan welcomes the location of these new Power Centers but, it also recognizes the necessity of new alternative configurations, stricter zoning, land

B. Location map (commercial intersection).

C. Picture of Highway Intersection.



C.

development regulations, and architectural standards. It is essential to understand that the addition of Live/Work units and defined public spaces may add life, capture trips, and produce spaces of pride and opportunity.

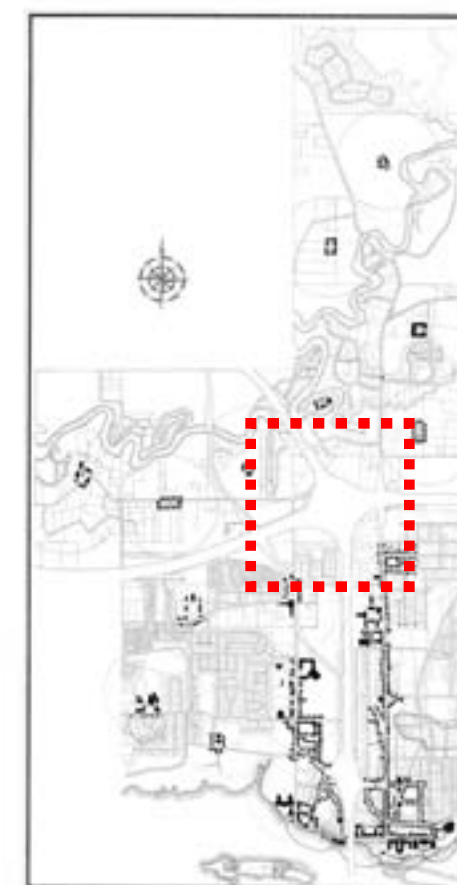
The citizens of D'Iberville must decide how much and what type of commercial development they would prefer outside of the downtown. Unless the City of D'Iberville creates a coordinated and complementary approach to growth management with Harrison County or, unless it pursues outright annexation of some of the areas along this corridor, it will have very limited ability to control its sprawling growth.

D'Iberville and Harrison County residents are expected to welcome some types of larger format retail but it must be well planned to satisfy long-term needs. For instance, there is an unmet need for home improvement good, home furnishings, discount retail (as Wal-Mart's arrival reveals), and many other types of goods and services; and, this might just be the beginning of what will be a much larger influx in the coming decade.

The areas outside of the downtown should be planned to include concentrated neighborhood, village, and town centers to accommodate retail that does not compete with the downtown and that provides a configuration different to the sprawling retail in "suburban

Genericana". Today, dozens of examples of Town Center projects might serve as paradigms for new land development regulations.

For instance, limiting the amount, quality, and type of development has been a successful strategy to complement existing and proposed uses in downtown areas. This type of strategy has been successfully used in the revitalization of the Third Street Promenade in Santa Monica as well as in Reston Town Center in Virginia. The only caveat is that, this type of strategy requires a proactive approach to growth management on the part of the D'Iberville's leadership.



B.

A. Location of
existing water
plant.

B. Location of
proposed water
plant.

C. Drawing by
Mayor Quave.



C.



A.



B.

Industrial Districts:

The relocation of the existing Water Management Plant to the proposed annexation area on the north side of the City provides a perfect opportunity for the development of a chain of servicing and light-industry districts in this area. The creation of a Seafood Storage and Delivery District could become extremely beneficial for the regional environment for it could very well make use of the Water Plant for the direct deposit of its industrial residues.

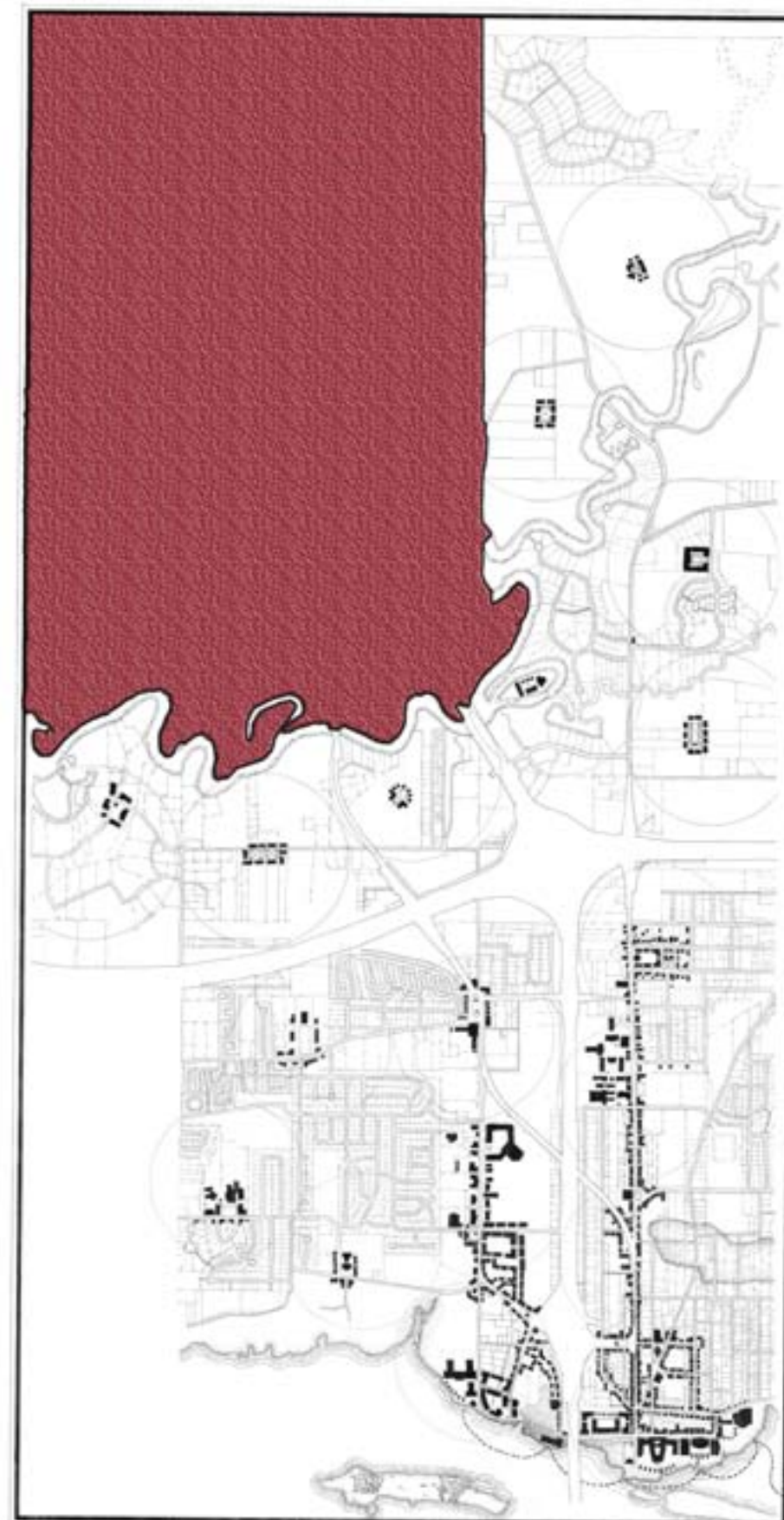
A regional Life-Style Center is also proposed on the South-West side of the Inter-state Highway intersections –across the existing FEMA temporary housing site. This small district could contain a program with one or several of the following activities: welcoming center, gym, bowling alley, museum, open-air theater, drive-in restaurant, movie theater complex, designer retail, etc.

Note: for the sake of land use diversity, urban richness, and security, it is important to introduce a minimum amount of Live/Work or housing units within these districts. They should never be monolithic; they should always contain a variety of land uses working in unison at various times of the day.

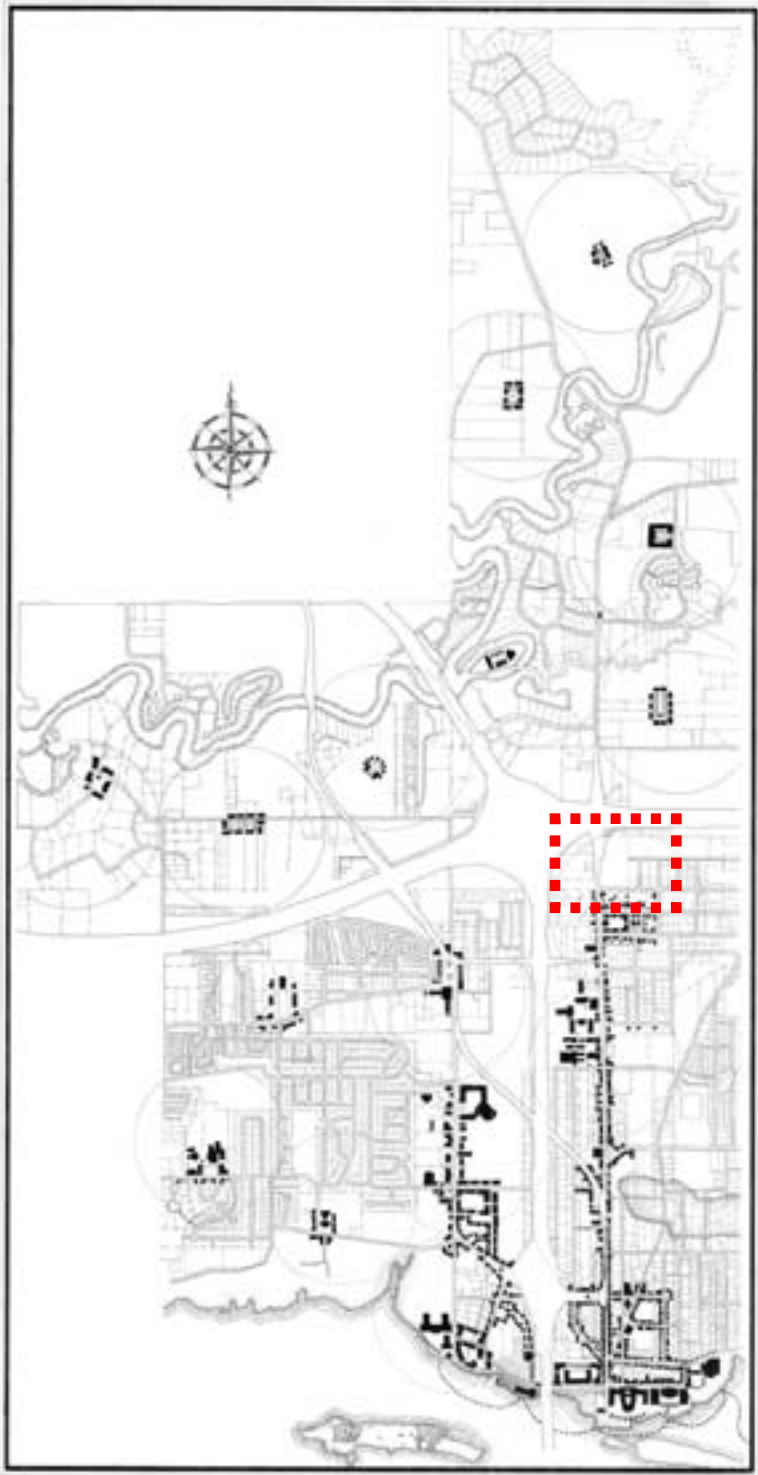
Annexation Proposals:

The cost-benefit analysis of any annexation proposal should always be based on issues ranging from urban economics to the establishment of potential political controls on unincorporated areas. In the case of the D'Iberville Region, the annexation of one single area on the North-West side of I-110 seems to be important –for the location of the new Waste Management Plant and Sea-

food Industrial District; this simple location decision would reduce the existing amount of bay water, surface water, and storm water pollution considerably. Nevertheless, the design team recommends an in-depth economic study by its City Officials before embarking on this new venture.



A. Location of
proposed
FEMW site
(tent city).



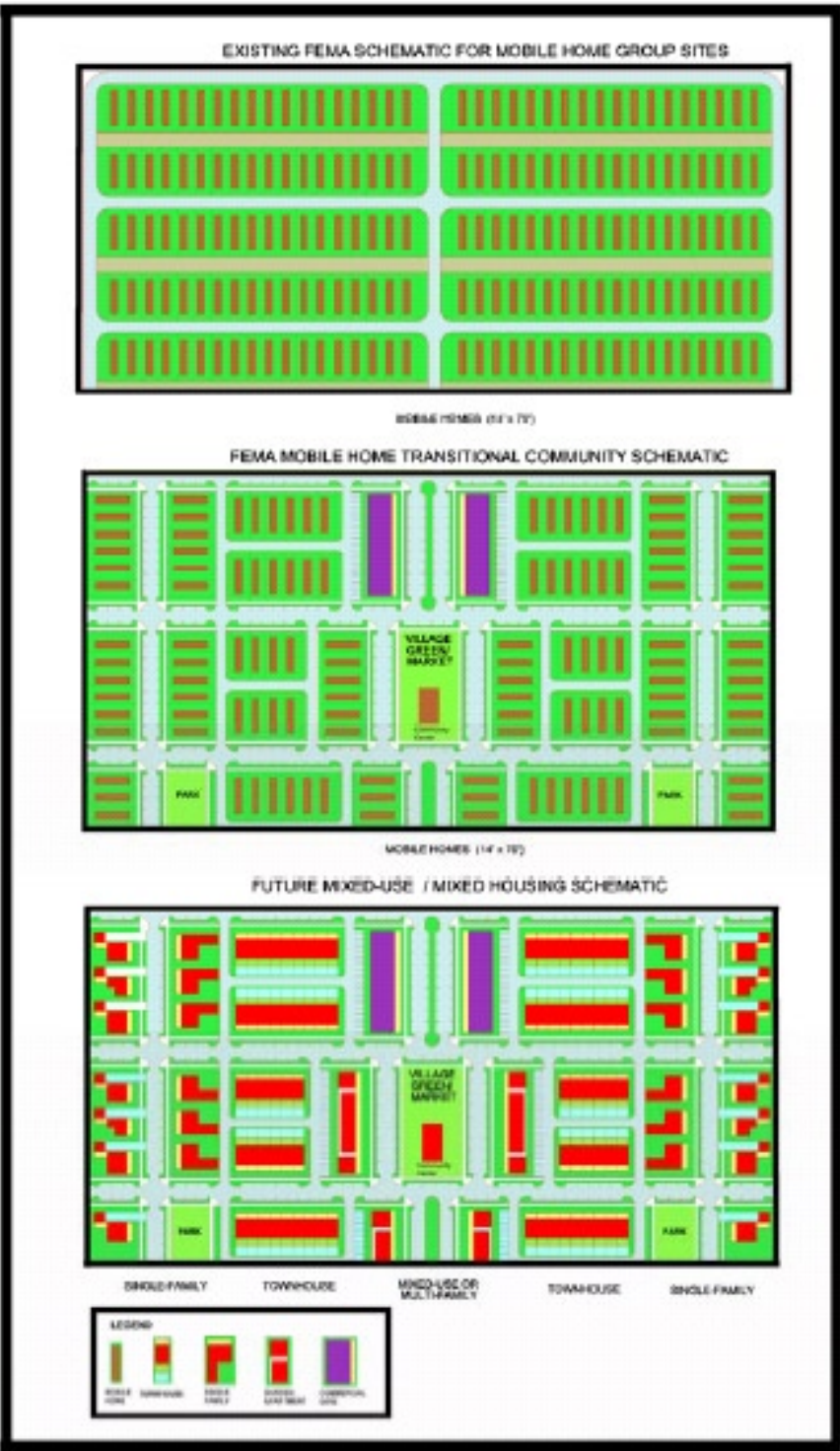
FEMA Temporary Sites:

FEMA has identified the current “Tent City” site on North Central Avenue as a potential case-study location for their new housing proposal. Their proposal pays for permanent infrastructure expenses if the site is designed to become a permanent location for a diversity of housing organized around a public square. A gradual housing transformation would occur; from trailers to temporary housing; from

temporary housing to permanent housing on lots of various sites.

For more information, please contact Jorge Quintero at the FEMA Housing Area Command in Baton Rouge, Louisiana or consult the FEMA Joint Housing Solution Center preliminary report titled: “Transient Communities Principles”.

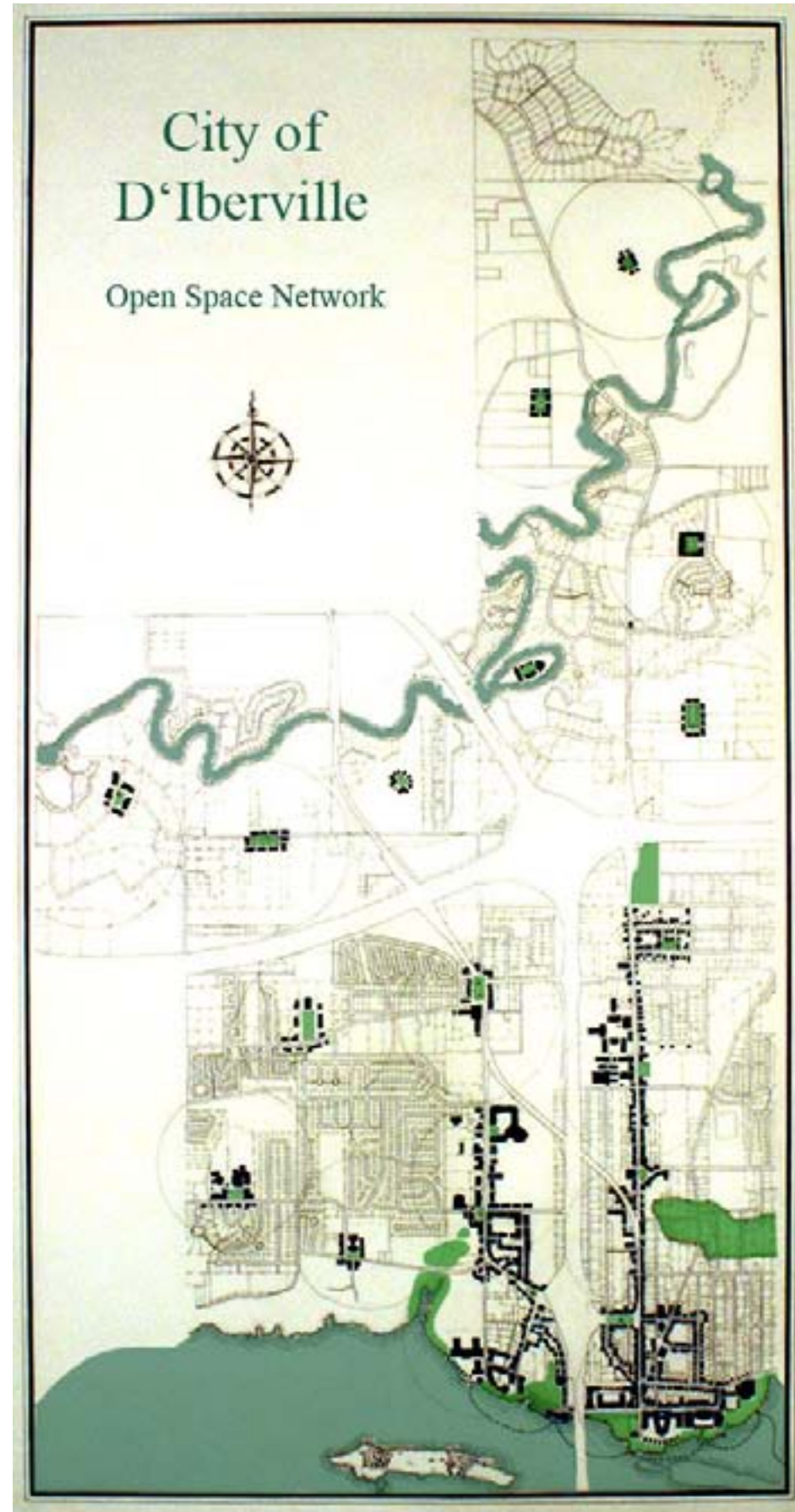
- A. FEMA proposal.
- B. FEMA prototype site in Baton Rouge, LA.



B.



C.



Open Spaces:

The regional master plan includes a variety of vehicular thoroughfares as well as a network of green spaces for the livelihood of the regional natural ecology. It shall be understood that, open space provides the greater part of the public realm at all scales, from the region to the lot; and that, it includes a wide range of types, from thoroughfares or roof gardens to greenbelts and farmlands.

In conventional development systems, open space is typically expressed as a ratio of some function, such as population or land

area. This practice may result in large, misplaced open areas, which are underutilized by the community. In fact, in conventional development systems, the only types of open space which are carefully considered are determined by either environmental regulations or by parking requirements.

Open spaces, to be effective at an environmental scale (including human beings), must be specialized in function, distinguished by size, landscaping characteristics, frontage requirements, equipment, and should be located in appropriate sites. Open spaces range from rural to urban and from watershed to patches.

Moreover, to be truly public, open spaces must be fronted by building facades, which is to say, circumscribed by thoroughfares. To be fully used, open spaces should straddle pedestrian and natural trajectories (including natural migratory patterns, nesting locations, etc.) or be adjacent to meaningful destinations.

Natural Landscape Network:

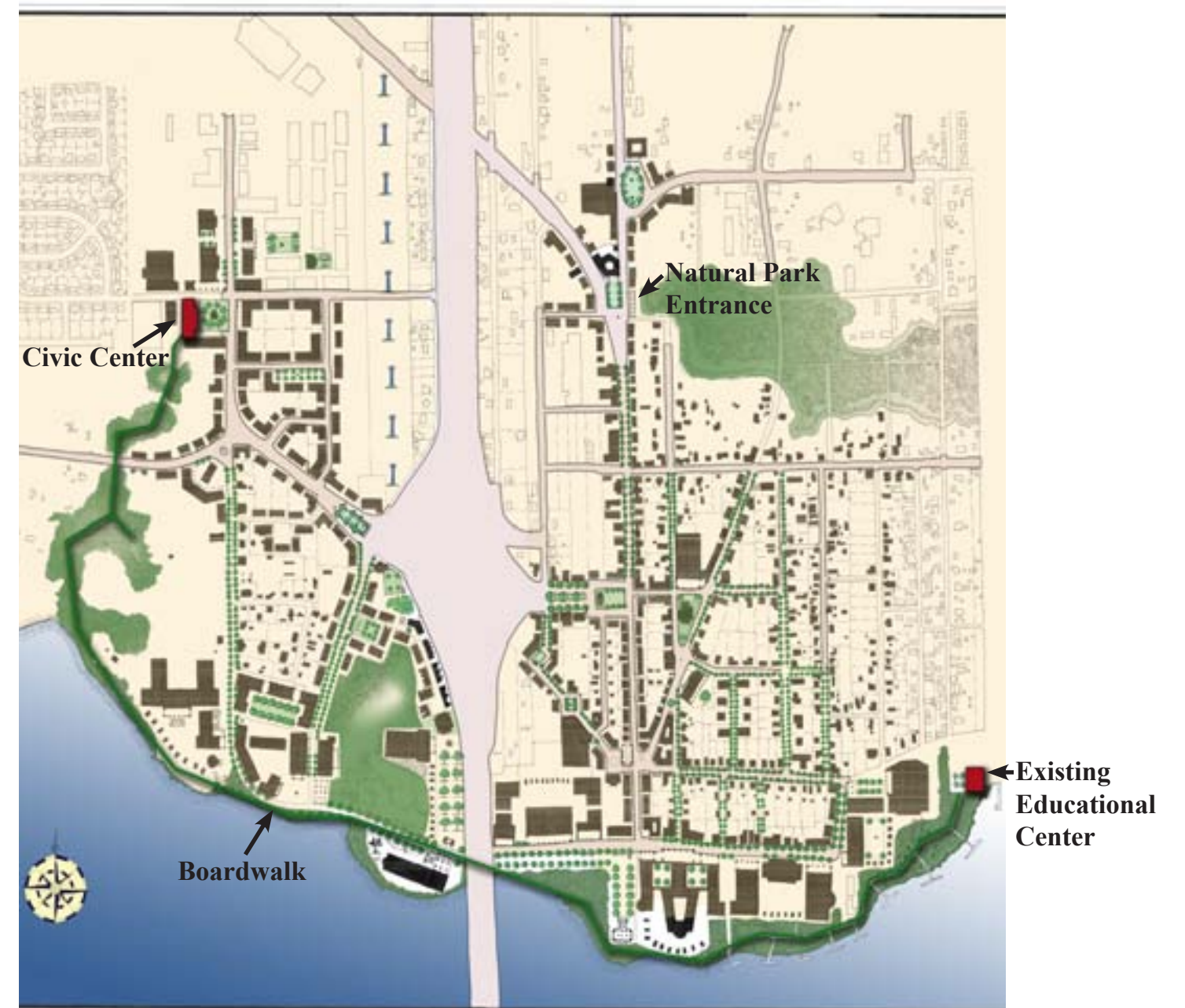
The regional plan provides a continuous network of undisturbed natural landscaped areas which traverses in the downtown area from the City Hall Civic Center Plaza to the Environmental Education Center, and from the Inner Bay to the newly created central space at the intersection of Central Avenue and D'Iberville Boulevard. The intentions are to protect these habitats from development in perpetuity and, to offer opportunities for the education of new generations on the importance of non-human intrusion, birds, exotic species, endangered species, local ecologies, etc.

Local officials should pursue the creation of partnerships with non-profit conservation organizations like NatureServe (www.natureserve.org) or the Nature Conservancy Network –which provide the scientific information and tools needed to help guide effective conservation action.

The natural landscape network is also a source for the conservation of water quality,

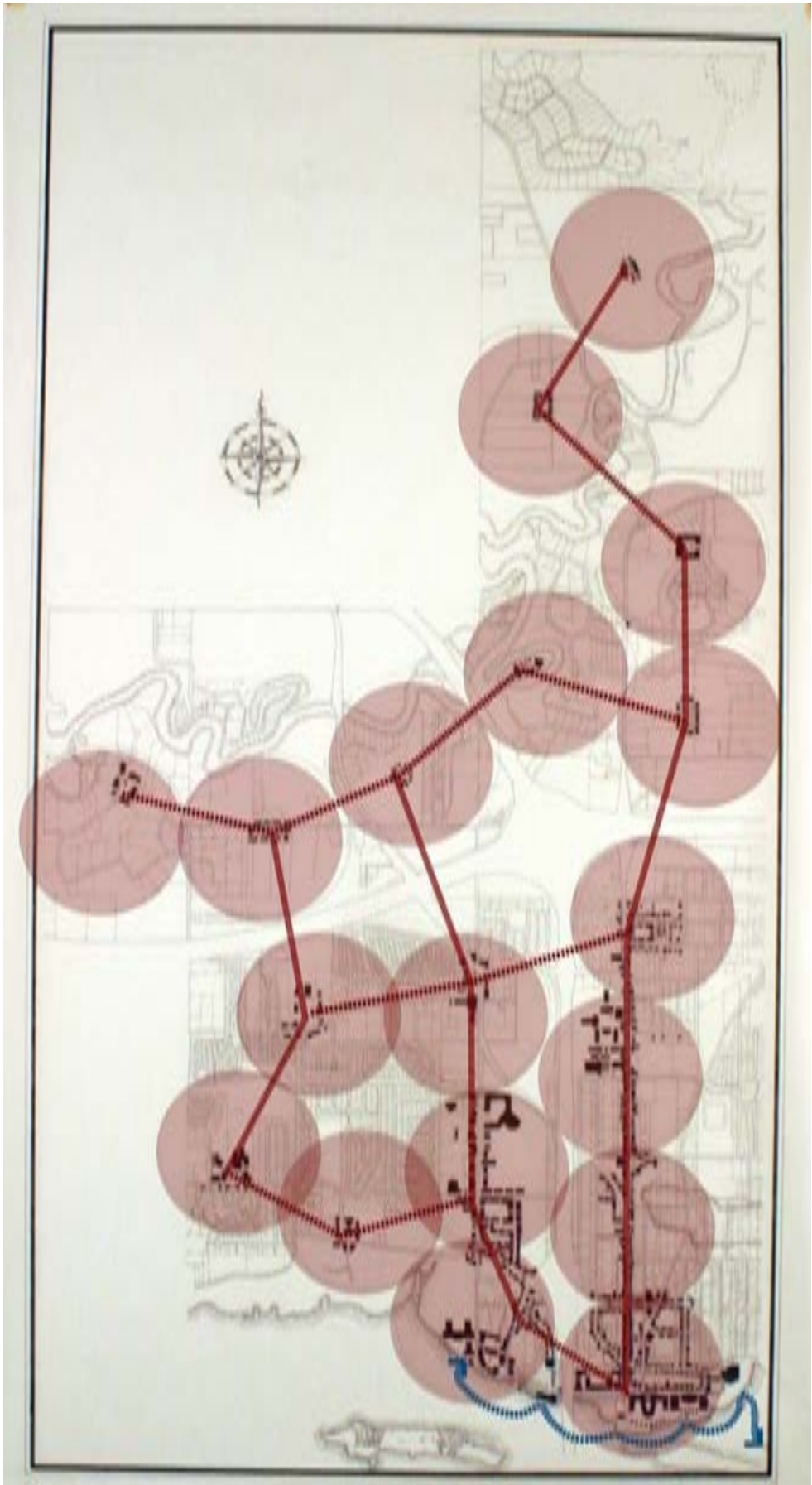
natural hydrology, and habitat through the conservation of water bodies, riparian areas, and wetlands. At least 60% of any on-site wetland, riparian area, or water body should be undisturbed; moreover, a 100 feet buffer should be created between encroaching development and any protected area. It would be important to emphasize the need to minimize site disturbances during building construction –particularly during the construction of hotels and casinos along the bay. If necessary, local officials should identify the limits of building areas through the creation of footprint zones coinciding with the ones proposed in the 2005 D'Iberville Citizens Master Plan.

The design team also discussed several proposals for farmland preservation which were dismissed given the emergency of the current situation. Nevertheless, local officials must make an effort to discuss the preservation of irreplaceable agricultural resources by protecting prime and unique farmlands and prime soil from unbridle development.



A. Local Transportation Network.

B. Detail of one of the Centers.



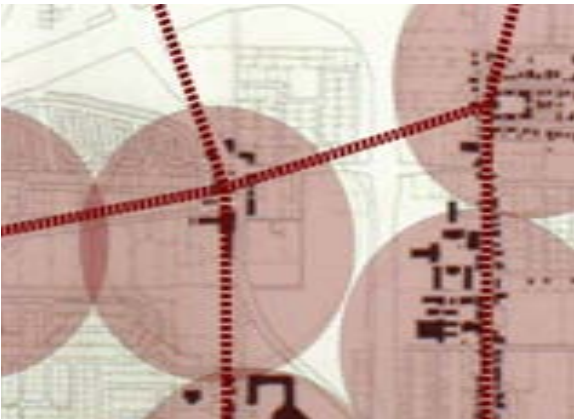
A.

Local Transportation:

The master plan promotes a network of local transportation as a threefold objective: first, to reduce air pollution, energy consumption, and greenhouse gas emissions generated by suburban transportation; second, to encourage pedestrian friendly environments; and finally, to reduce the amount of trips generated by unnecessary automobile usage. A small

Jitney or a make-believe tram/mini-bus could serve every neighborhood center.

Of the outmost importance is the creation of a first local transportation line between the proposed French Quarter and the existing Wal-Mart area.



B.

Water Taxi:

A proposal for a water taxi was put forward between the design team working in the City of Biloxi and the one working in the City of D'Iberville. Amongst its immediate advantages, a water taxi would be beneficial for the hotel and casino tourist population and the elderly; it would create a network of connections on the two sides of the Bay; it would augment the traffic potential of particular projects within the Cities of D'Iberville and Biloxi; it would promote the preservation

and conservation of the existing ecology; it would allow citizens and visitors to admire the historic landscape; and, it would add an extra sightseeing mechanism to enhance various tourism programs within the City of D'Iberville.

As proposed, the water taxi would have strategic stations at the various hotel and casinos along the water front.



Detailed Master Plan

Great downtown areas are characterized by many distinctive streets and public spaces - each with its own character and combination of activities. The detailed master plan proposes five mixed-use sectors with particular characters including: the waterfront casino row, the new French Quarter on Central Avenue, the live/work district, the civic neighborhood, and the old historic areas.

The public officials of the City of D'Iberville must be aware that a mixed-use development

is not only about economics but about life and place-making—creating spaces with attractive destinations for time well spent. In general, people do not come to downtown areas for everyday shopping needs; they come looking for an urban experience including: entertainment, an attractive waterfront, a variety of historic and contemporary architecture, pedestrian friendly areas, lots of things to do, see, explore, meet, etc.; most importantly, a downtown is NOT an artificial environment but, a friendly public stage where people go

to see and be seen by other people.

People come to central neighborhoods and main streets to have the “Communal Experience” they cannot find in a mall or in a strip center. For the City of D'Iberville to be successful it must deliver this experience, create this positive image, and market widely this type of unforgettable feeling. It only takes a few steps.

Now is the time.

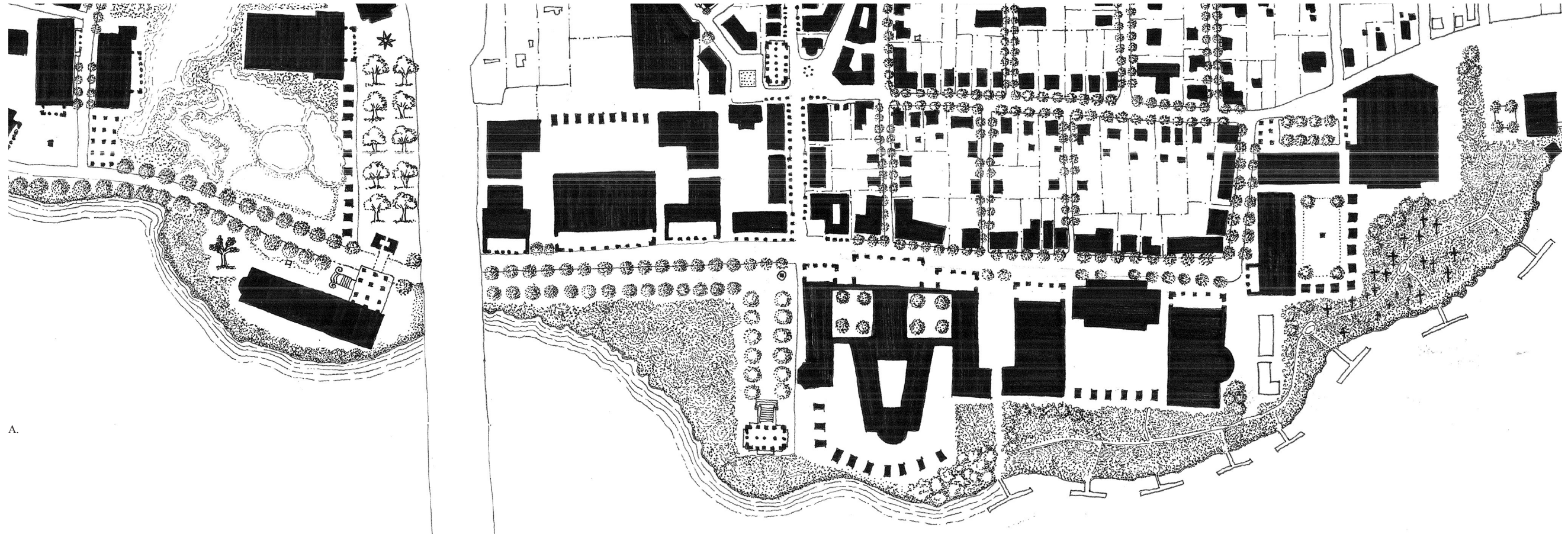


A.

A. Detailed land use master plan.

A. Casino Row detail.

B. Casino Row location map



1. Casino Row:

The residents of D'Iberville do not want suburban-style shopping centers, big box retail stores or a large number of chain stores and franchises in their historic downtown area—even when there are no historic buildings left.

By pure observation, and in accordance with trends established before the hurricane devastation, the south side of D'Iberville has four niches and retail development strategies

that can be used for its successful reconstitution: waterfront uses, entertainment, arts and culture, and live/work areas.

The future of any retail strategy must focus on the continuation and development of: casinos, hotels, restaurants, home businesses, core galleries, cafes, and businesses geared towards entertainment and the interests of the arts and culture community. Moreover, the addition of key underserved market nich-

es would highly complement such a retail mix—including home and design-related facilities.

Casino Row is a mixed-use district located along Bay Shore Drive between the proposed extensions of Santa Cruz and Fournier Avenues. The buildings are flanked by a continuous esplanade between Santa Cruz and Central Avenue and protected from the Inner Bay by a network of wetlands and parks for the enjoyment of tourists and residents alike.



B.



C.

Each casino has been designed to follow a precise typological concept: no building or part thereof should be longer than 300 feet. Given the normal scale of these buildings (particularly their hotel components), the 300 ft. limitation in length would avoid the creation of huge and incompatible mega-structures within a neighborhood of small buildings. At the same time, each compound is divided into architectural objects that can be named and recognized by their basic components: casino, hotel, cabanas, and recre-

ation area. The casinos shall not exceed 65 ft. in height; the hotels shall not exceed 30 stories; and the cabanas should be designed like small viewing towers.

FEMA requires these buildings to be raised approximately 24 ft; therefore, parking garages may be accommodated under the main structures of the casino and hotel areas. Nevertheless, parking garages shall be lined with program on their public side to enhance

their pedestrian vitality i.e.: temporary retail for tourists (T-shirts, sun glasses, postcards, etc.), food joints, or any use which can be rapidly dismantled or washed out during the next 100 year storm.

A beautiful loggia at the corner of Central Avenue and Bay Shore Drive will commemorate the foundational moment and will provide amazing view of the Inner Back Bay and the City of Biloxi.

A. French Quarter location map.

B. French Quarter view.



A.

2. The French Quarter at Central Avenue

South Central Avenue should become, once again, the center of gravity of the whole region. In its post-hurricane state this area is completely devastated by the storm surge and very few buildings are still standing; and, in its pre-hurricane state, South Central Avenue was NEVER conducive to a good pedestrian experience -the relationship between building frontages and continuity of sidewalks was not parallel or concurrent. In theory, the more continuous the building frontages along uninterrupted sidewalks, the more energy, livability, and aliveness the retail place will have; and vice versa, the less continuous the building frontages along interrupted sidewalks, the more lethargic, dead, and ungraceful it will be.

In D'Iberville, our empirical observation of this relationship proved to be quiet disturbing. The continuity of the building frontages was usually absent. There was no ordering



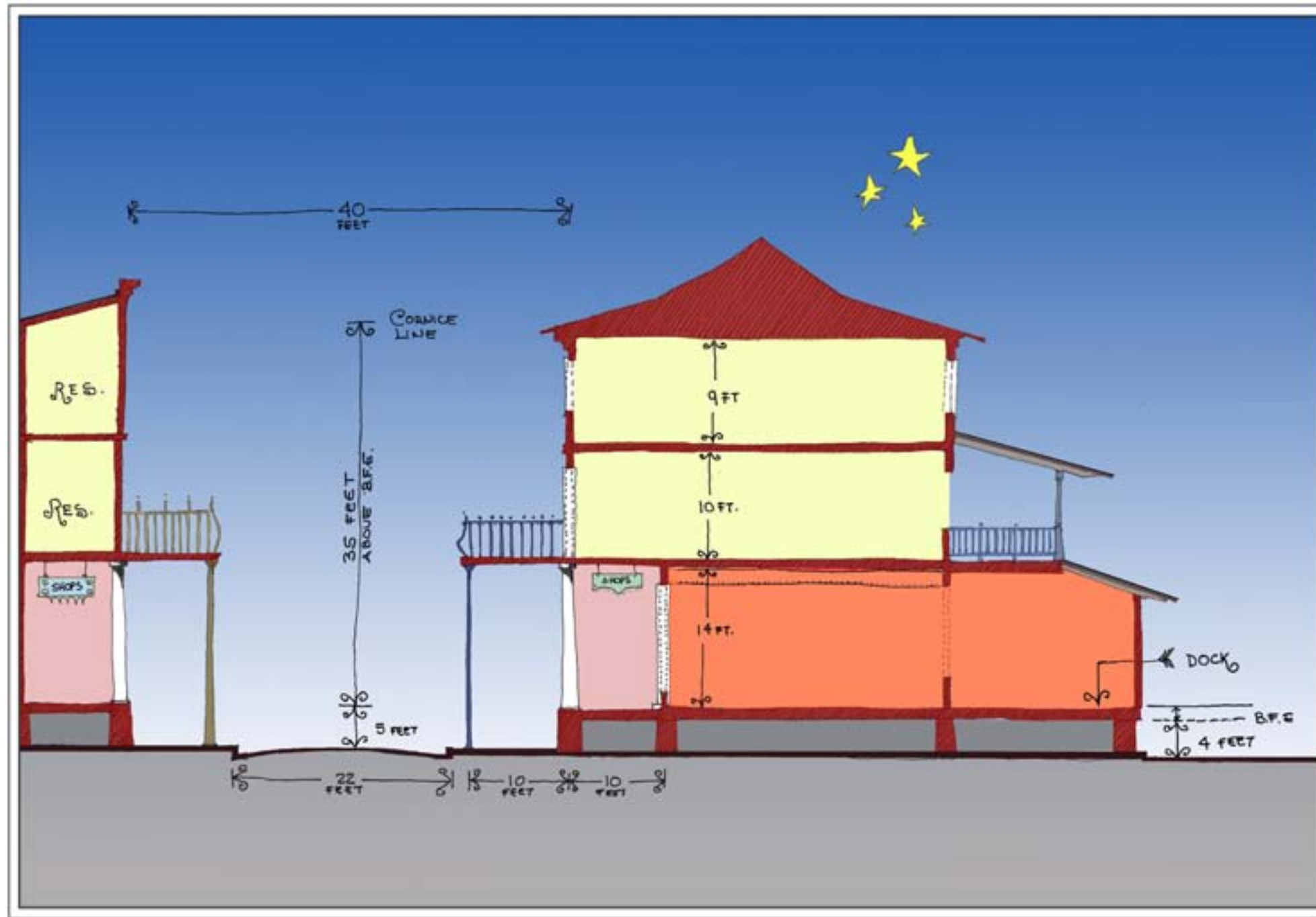
B.

principle that could be reconstituted. As a consequence, and in order to avoid the lethargy of a boring indefinite street, the design team decided to create incremental areas with distinctive characters along this mile of opportunity. New supportive land development regulations (LDR's) and architectural guidelines, emphasizing these various conditions,

should encourage the production of pedestrian friendly urban buildings of the highest design quality.

The original French/Spanish character of D'Iberville should become the foundation for vibrant retail along South Central Avenue. The development of its compatible land

development regulations is essential for the success of any development endeavor. New buildings, with contemporary uses, must create life, must be compatible with its neighbors, and must bring back the most beloved elements of an old/traditional mixed use downtown sector.



c.

Since FEMA requires the buildings in this area to be raised at least 4 ft, the design team has proposed a street section modeled on a traditional syncretism of New Orleans and Miami Beach buildings with a combination of arcades and raised porches. This emerging type has no American precedents.

This is the place where D'Iberville comes to eat. New town centers and main streets have learned from the “restaurant rows” forming naturally in cities; by concentrating restaurants on a single street, a few blocks, or around an attractive public space a dramatic increase of customers may happen—in gen-

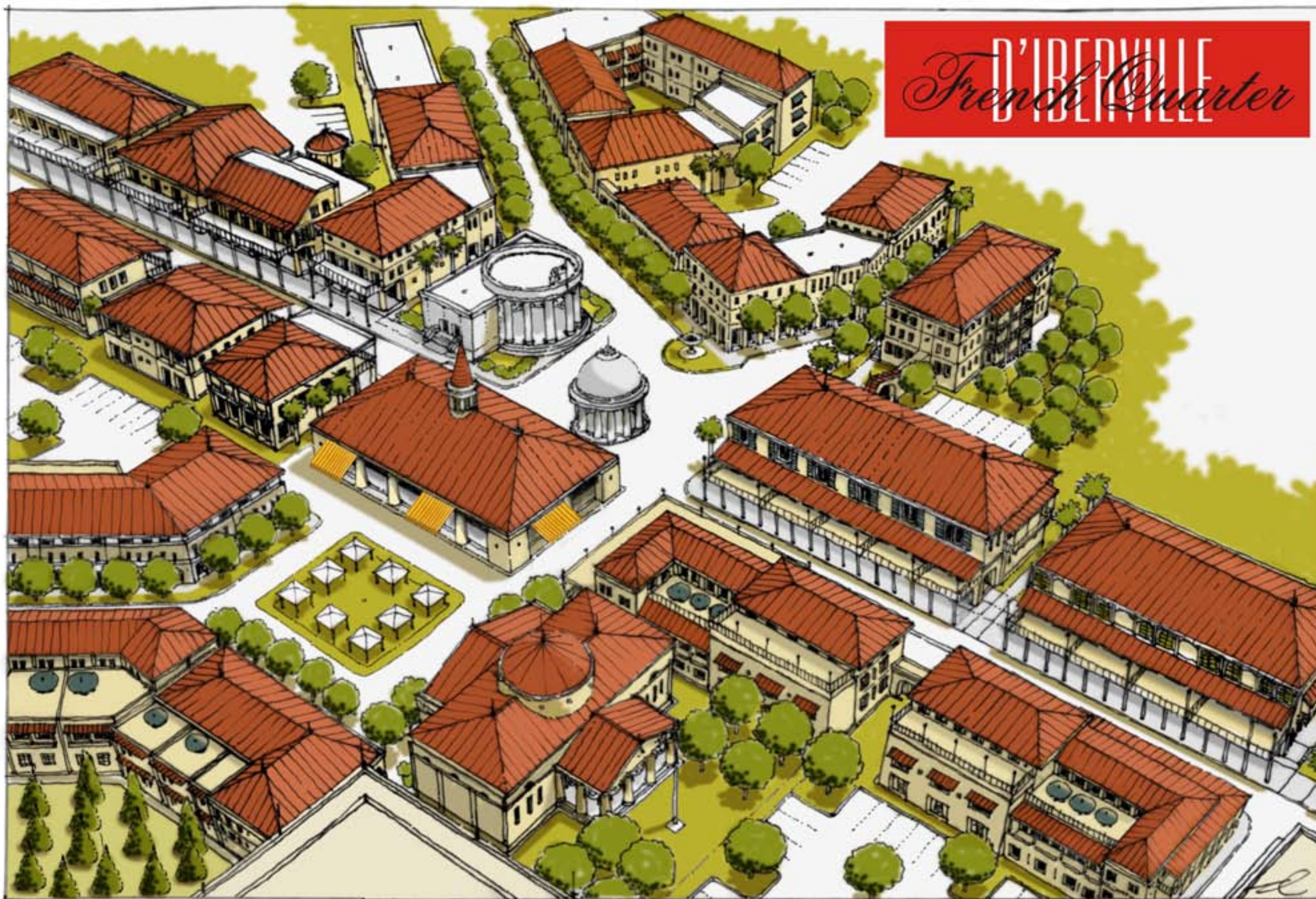


C. French Quarter section.
D. French Quarter plan detail.

D.

eral, the density of restaurants is correlated to the attractiveness of a place as a dining destination. For instance, in the Mizner Park project, in Boca Raton, shops and restaurants surround an attractive public space and people stroll and shop all around the boulevard plaza, underneath a proportional arcade, while waiting for a restaurant table.

Complementary shops and businesses akin to Williams and Sonoma (or a local independent version of this), floral shops, wine bars, cigar stores and other complementary businesses could occupy retail space. This is a lively part of town and upper floor space would become an attractive residential location for people who enjoy living close to the urban action or to their own offices (Mizner Park has both).



D'IBERVILLE
French Quarter

The Open-air Market at Race Track Road:



B.

The master plan proposes the location of a civic space at the intersection of Race Track Road and Central Avenue. This new place would contain a band stand at the termination of pedestrian vistas, a couple of opportunities for the location of important civic buildings (chamber of commerce, mini-police station, etc.) and, most importantly, a formal and an informal open-air market.

Open-air markets may be programmed to suit the preferences of downtown merchants associations and residents. Typical activi-

ties may include: farmer's markets (fresh produce), arts and crafts, antiques, flowers and other nursery items. The goal should be to establish a regular market that is professionally managed with a strict screening of tenants, programming of special events and entertainment, and marketing much like the Main Street Program practices for a downtown.

Open-air markets also provide an inexpensive way to jump-start potential downtown businesses – freestanding, open-air colonnades

are much cheaper to build and cost nothing to operate compared to air-conditioned buildings. In fact, open-air markets create terrific business incubators. They provide new merchants with an inexpensive way to try out small business concepts without purchasing property or leasing more expensive interior space. The managing organization should work carefully to support tenants, provide small business advisory services, and help successful merchants transition into permanent space in existing and proposed buildings within the downtown area.

Critical Mass of Retail and Center of Gravity:

The more retail and entertainment is assembled in one place, the more attractive a destination becomes. This rule is the same for shopping centers, strip centers, and downtown areas. The greater the variety and concentration of retail, the larger the trade area becomes – the greater the distances people are willing to travel.

Larger trade areas bring more customers and create more successful retail. The Wal-Mart Mall, for instance, has succeeded because it has a concentration of retail shops, home repair shops, hotels, tire places, burger joints,

and restaurants, a relatively attractive location, and a “brand name” image that lends itself to marketing. Many downtowns, however, do not take advantage of these opportunities. As a retail destination, the City of D’Iberville could grow stronger with the addition, in quantity and quality, of: shops, restaurants, arts and cultural activities, etc.

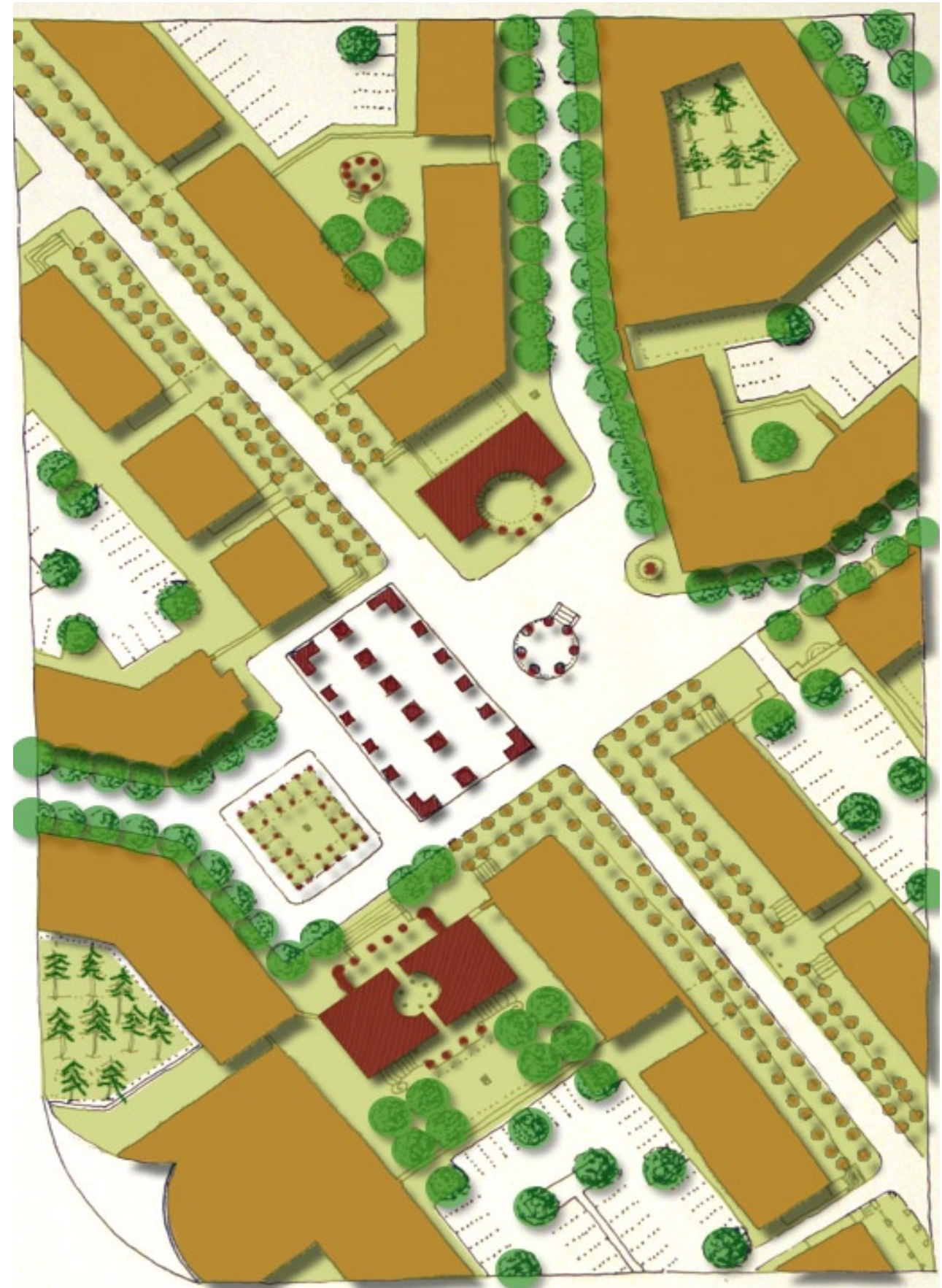
The first order of business should be the creation of a core destination within the South Central Avenue and the Bay Shore Drive areas –a center of gravity; a place where a critical mass of shops, restaurants, and enter-

tainment venues would complement the creation of public space. In terms of place-making, this requires completing the two sides of a street or the four sides of an open public space (i.e.: proposed plaza and open-air market at the corner of Race Track and Central Avenue) with retail activity.

Note: Vacant lots, empty window fronts, parking lot frontages, drive-in tellers, and crossing fast one-way streets should be avoided in these areas -for they are not pedestrian friendly or conducive to retail activities.

Coordinated Leasing, Management, and Marketing:

Although downtowns are not shopping centers, they do need to adopt the coordinated leasing, management, and marketing practices that make shopping centers successful. These practices have been translated and adapted by the National Main Street Program to serve downtowns where there are multiple property owners and merchants. The existing merchants associations, the D'Iberville Chamber of Commerce, and the City of D'Iberville should seriously consider creating a Main Street Program (MSP) and hiring a Manager to implement it.



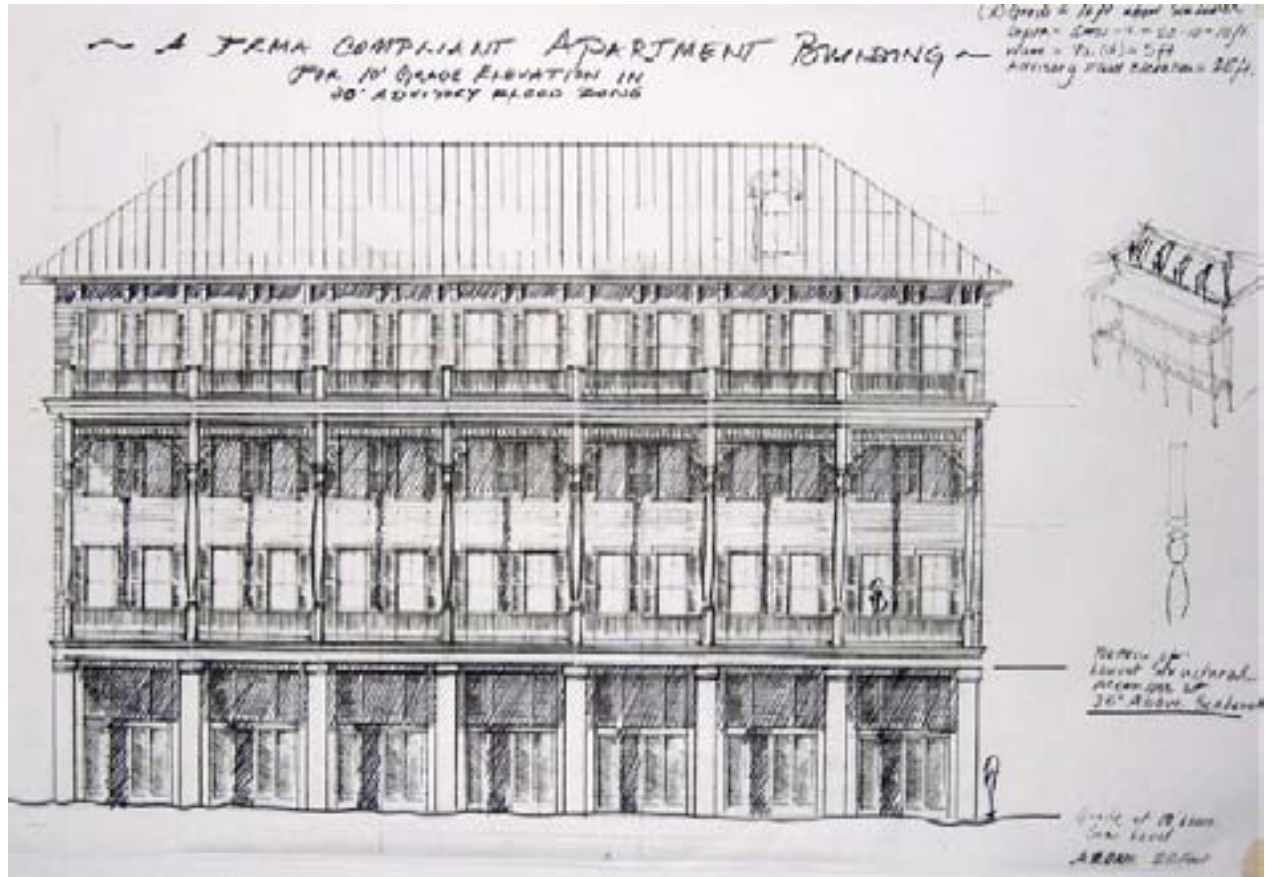
A. Live/Work district location plan.

B. Four story Live/Work unit (retail under townhouses)

Drawing by CNU Architecture Team



A.



B.

3. Live/Work District

Located between Rodriguez Street and Lemoyne Boulevard, this mixed-use district requires live/work units –a type of housing unit which, by definition, requires a combination of living facilities (typically of a lofty type) and work areas (typically for no more than 3-4 employees/unit bay). The configuration of Live/Work units comes in three types:

- 1. Work on the ground floor and living facilities on the upper floors,
- 2. Work in the front of the lot and living facilities in the rear, and
- 3. Living facilities in the front of the lot and working area in the rear.

The Squares at Rodriguez Street:

In addition to live/work units, the D’Iberville Regional Citizens Master Plan identifies the setting and location of unique gateways and landmarks announcing regional territorial boundaries and thresholds. These gateways and landmarks must be understood as economic, efficient, and beautiful pieces of architecture, land art, or sculpture which carry a high content of symbolism.

The charrette team proposed, as a mere example, two of these pieces on Rodriguez Street at the exits of Interstate Road 10. These pieces were designed as small plazas with sight-seeing belvederes. The design team also sug-



C.



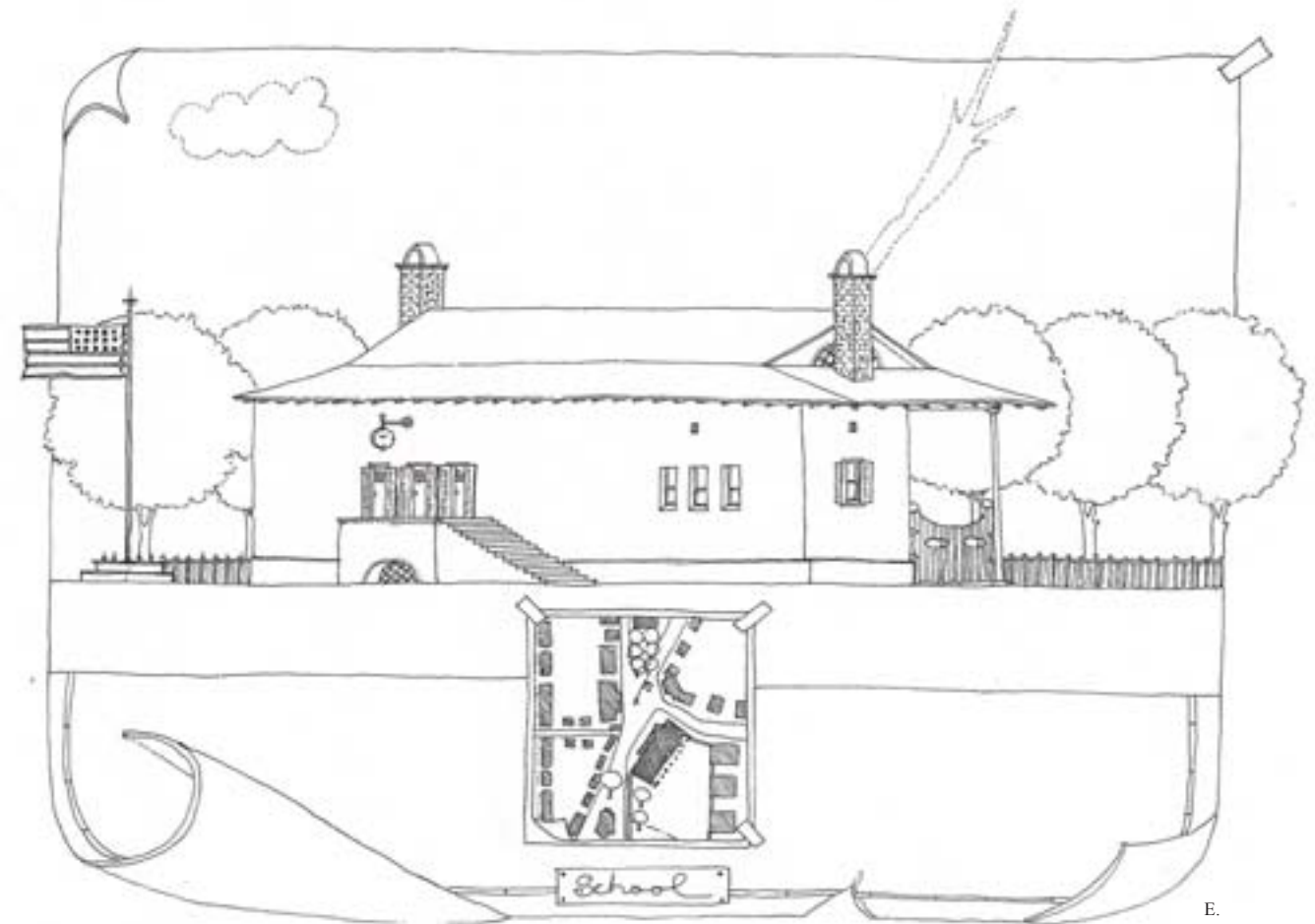
D.

- C. Rodriguez Squares location map.
- D. Magnet School location map.
- E. School proposal.

gested covering the existing water tanks with a greater degree of ornamentation -including the name “D’Iberville” in big colored letters, a weathervane, and other elements inspired by those one may find in this area of Mississippi.

The Magnet School and Gallery:

A third gathering place and cluster of synergistic businesses is envisioned for a public space in front of the old Middle School Building at the corner of Church Street and Gorenflo Avenue. The School Building could either be partly rehabbed for a library or some combination of arts and cultural center. A new magnet school and a row of townhouses and apartment buildings could occupy the rest of the site. In the middle of the historic church park, a new place of worship or an art gallery with art-related businesses could flourish. This area is planned as a central gathering place for the artistic and cultural lives of the residents of D’Iberville.



E.



**Retail Center at Central Avenue and
D'Iberville Boulevard:**

An existing cluster of retail businesses is already established at the intersection of D'Iberville Boulevard and Central Avenue –at the site of the Clock Tower. This center could provide everyday retail goods and services and could be connected to the rest of the downtown core by reintroducing retail services along D'Iberville Boulevard and by extending the high quality pedestrian character of Central Avenue.

The existing storefronts and parking in front of the surviving buildings is not ideal for downtown retail; the huge gaps created by the parking lots in front of the existing buildings would need to be remedied to truly make this an attractive pedestrian extension from the downtown core. Short-term solutions may

include very thin Liner Buildings (Father-Son-Holy Ghost Types) to screen the parking lots. Long-term solutions may include the construction of a parking garage to free up land for more substantial infill. In fact, a parking garage could create an opportunity for shared parking arrangements between the residential uses, active during the wee hours of the day, and types of retail uses that might be more active during working hours.

A network of high-quality pedestrian streets can be staged starting at Bay Shore Drive and Central Avenue and ending at D'Iberville Boulevard. These gathering places, viewed collectively, will make a much richer downtown than could possibly be achieved by any one of them individually.



B.

New Connecting Loop:

It is not a secret that, with the creation of the desired Looping road between Popp Ferry Road and Lemoyne Boulevard both sides of these roads will change radically within the next decade. This fact presents an opportunity to completely revamp the character of these two critical thoroughfares and of North Central Avenue.

A lot of it may be accomplished simply by pulling buildings close to the street and by improving sidewalks and lining these with trees in planting strips. The addition of lighting at important intersections and the design of the two traffic rotundas will announce drivers they are entering a very special place; street trees will cause them to slow down. Traffic calming can be accomplished through the resurfacing of intersections with pavers

and through the provision of parallel parking at off-peak hours. Both of these two alternatives provide cues as to the pedestrian-oriented character of this area; plus, it would force drivers to slow down.

In terms of retail along these corridors, the market is wide-open for home furnishings and interior design-related businesses; it is clear that the entire region will be rebuilding homes, stores, and offices and replacing interiors. These types of businesses as well as good antique shops could add a core retail sector to the downtown. Some of the development along the “Designer Row” corridor could also go up to 2-3 stories and incorporate housing to calm traffic even further and to bring permanent residents to the downtown.

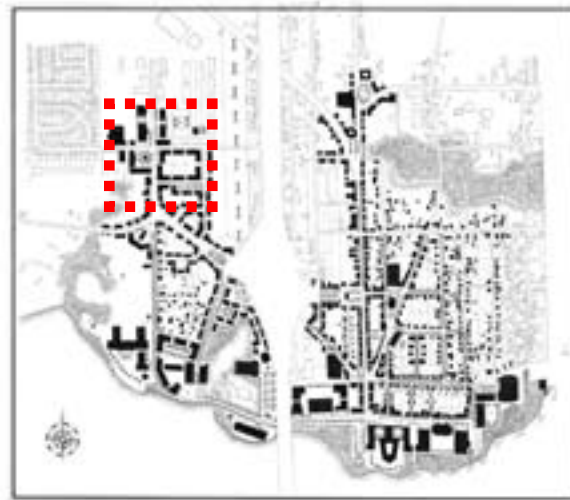


B. *Connecting Loop location map and new route.*

C. *View of one of the Loop Road rotundas.*

A. Civic neighborhood location plan.

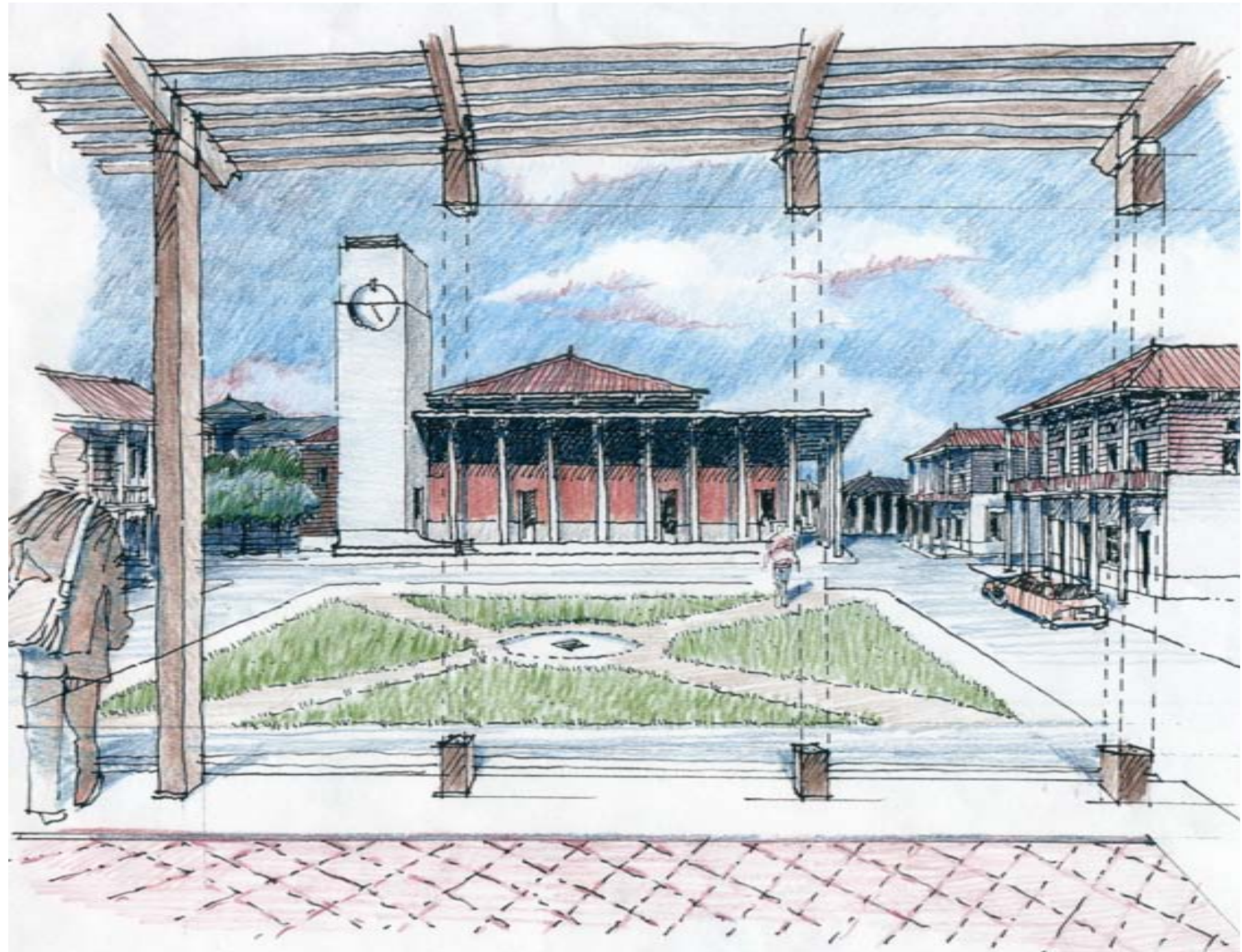
B. View of the proposed Civic plaza.



A.

4. CIVIC NEIGHBORHOOD

The City of D'Iberville should not force all of its civic buildings, parks and waterfront areas to be single-use islands. Place-making relies on the synergy of people going to libraries, concerts, art shows, and waterfront recreation in conjunction with dining, shopping, and commercial entertainment. For instance, the bistros and restaurants along one of the sides of the Mediterranean revival church in City Place, Florida, or Bryant Park in New York City could be understood as two analogical paradigms. In these two cases, there is a good synergy amongst civic/cultural buildings, dining, and great public spaces.



B.

In the City of D'Iberville, the City Hall, the Post Office, the Library and other of its quasi-civic uses should blend with the arts,

culture, entertainment, dining, the cafes and the shops. Under all circumstances, the City shall avoid single-use islands with very lim-



C.

ited hours of operation. Never assume that it cannot be done; many other cities have already figured out how to make it work. Think

“CAN DO” and research what legal and intergovernmental arrangements are required. Don’t accept “NO” for an answer.

Residential Super-Blocks:

The provision of a critical mass of residents around the civic center is crucial for its vitality. Given the existing situation, the conversion of the auto dealer site owned by Ford Motor Inc. into apartments is essential for the success of this area of the plan. The master plan proposes an interconnected system of blocks and streets with pre-determined frontage lines –which allow for the creation of public spaces. As a typical super-block, the communal services would be located in the interior of the block i.e.: recreational areas, tot-lots, laundry services, parking, etc.

A. Detached
single-family
buildings
location.
B. Detached resi-
dential detail
plan.



A.

5. SINGLE FAMILY NEIGHBORHOODS

The City of D'Iberville had some of the most eclectic residential neighborhoods in the Mississippi Gulf Area. Its historic importance encompasses more than three hundred years of residential and civic experience. Nevertheless, the residential neighborhoods are currently under severe stress of new construction incompatibility.

Under FEMA flood regulations, new buildings must be raised to comply with Federal policies within V-Zoned areas. Therefore, the new Land Development Regulations must include design guidelines to avoid the production of the typical “mega-mansion on concrete poles” with parking underneath. The new LDR’s must emphasize the gradual raising of the houses by means of porches,



B.

terraces, loggias, courtyards, and other similar design devices.

Strong remains of the traditional landscape scheme are still evident within the public grounds of the residential district. The 2005 D'Iberville Citizens Master Plan suggests the over-stressing of these archaeological remains (i.e.: fixtures, benches, planters, etc.) and the production of inventive landscape

schemes with an emphasis in color, paving materials, texture, tree spacing, and similarities and idiosyncrasies.

The City of D'Iberville must create an incentive program for residents and developers of new and existing projects (for further information please consult the Downtown Delray Beach affordable housing programs in the State of Florida). For residents, it should

implement programs to subsidize down payments and promote residential ownership –to increase the level of maintenance and pride. For developers, it should help them reduce their cost of construction by allowing them to have density bonuses based on specific performance criteria; and, it should also establish public/private partnerships with the existing Housing Agencies and with other County Agencies that could benefit with the



C. View of residential neighborhood.

Drawing made for Pattern Book by David R. Csnor, Urban Design Associates.

C.

newly created real estate boom. The 2005 Citizens Master Plan must promote infill and new development compatible with the type of traditional housing increments that have characterized the City of D'Iberville during its long history.

Affordable Housing:

Affordable housing shall be integrated seamlessly into the neighborhoods. Small projects tend to be more successful than projects with large concentrations of units. The 2005 D'Iberville Citizens Master Plan attempts to restore the fabric of the traditional affordable city with a combination of apartments, town houses, detached houses, live/work

units, communal amenities, and neighborhood commercial uses located around public squares. Contrast these projects with the existing pods on adjacent areas: one with only apartment buildings (100%) and the other with multi-family units or duplexes and apartments (65%). It is obvious that, the degree of aliveness of the proposed incremental project is greater than the apparent life of the two other projects.

The City of D'Iberville must be flexible in its zoning enforcement and concentrate on design issues. As part of this emergency, it is vital to implement a pre-permitted set of housing types –to make things easier for the general public. Once developers understand the high quality of the first two or three projects, others will certainly follow. But, the toughest part is to enforce a desire for quality at the very beginning.

The Details

For the short term, the City of D'Iberville should focus its street landscaping expenditures on installing durable street lamps, benches, tree wells, manhole covers, garbage cans and planters (hanging or ground-based). Durable items made from high-quality and structurally sound materials -even when it means to postpone some resurfacing of streets and planting strips with pavers.

The downtown street trees should be carefully replanted with a caliper of no less than 8 inches and a height, at planting time, of no less than 8 feet. Moreover, these trees must be carefully selected in consultation with a historian and an arborist to ensure that the trees will create a canopy where desired. Avoid unnecessary obstructions of storefront displays, signage, awnings or lighting of the visual merchandising.

Finally, street trees can create landscape themes along particular streets –palm trees on the waterfront, flowering trees around the arts-related activities, canopy trees around the pedestrian areas – that give each street and park a distinctive identity (Coral Gables, Florida is a prime example of this type of development).

In general, store front display, signage, lighting, and landscape standards should be appropriate to a downtown main street environment providing appropriate standards and guidelines without preventing creativity, flexibility and expressiveness. Eclectic mixtures are the norm for main streets; therefore, a lot of care must be imposed to avoid uniformity of standards. Uniformity of standards may make the downtown main street look too much like a commercial strip center.

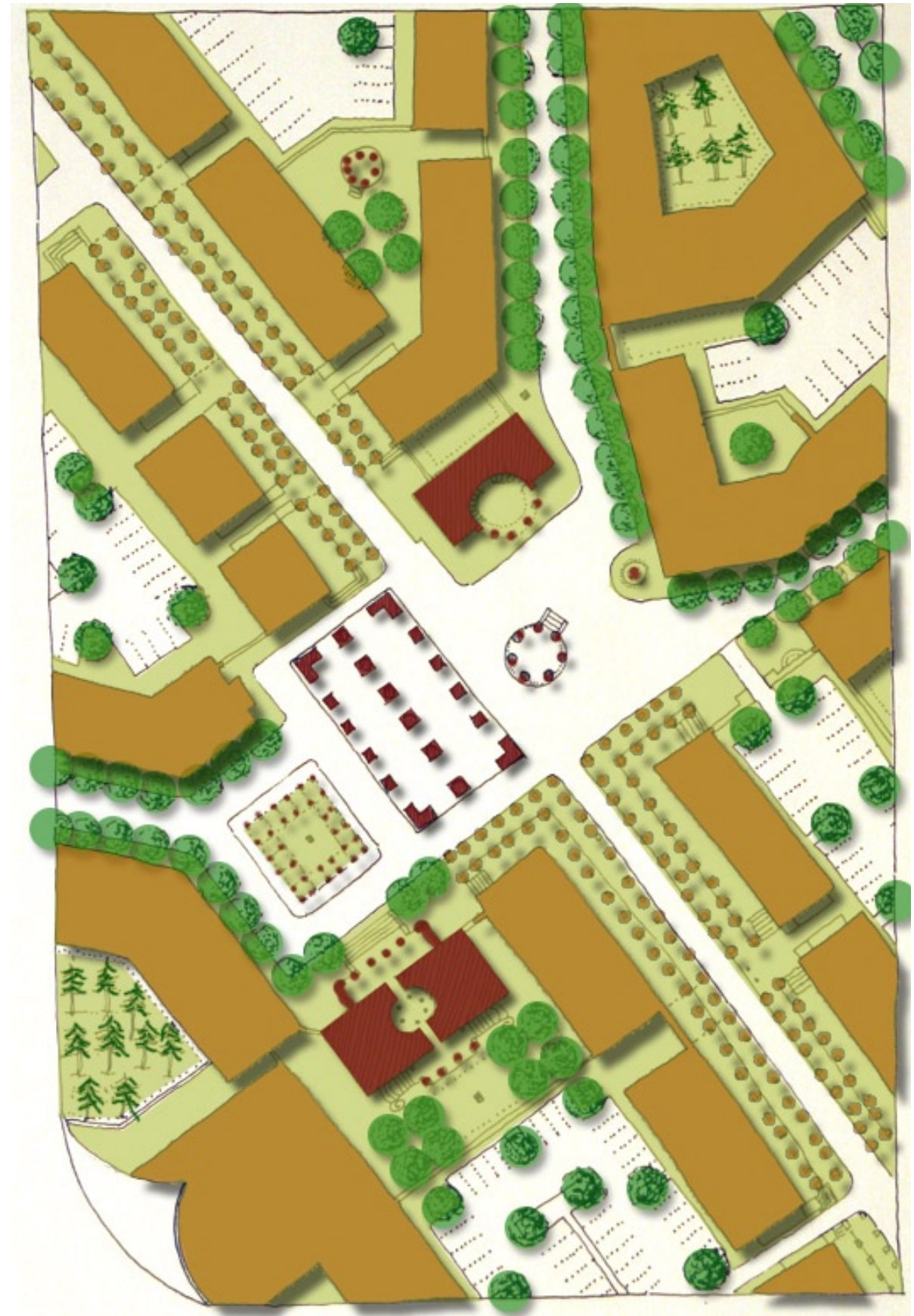
Note: suburban landscape treatments such as hedges, boulders, berms, “ground cover” plantings, post-and-beam fences and railroad ties should be prohibited, as should suburban street lighting such as cobra heads lighting commonly found on arterial roads and shopping center parking lots. All standards should conform to the very best elements of landscaping, lighting, signage and street furnishings found on great main streets, urban parks and plazas or, at least, the City of D'Iberville should stick to the minimum standards found in historic photographs of its old commercial area.



A.



B.



C.

- A. *Urban Retail Center.*
- B. *Civic Neighborhood.*
- C. *French Quarter.*

A. Raised domestic architecture.

Drawing by CNU Architecture Team.

B. Pattern Book cover page

C. Pattern Book inside pages.

Pattern Book by Urban Design Associates.



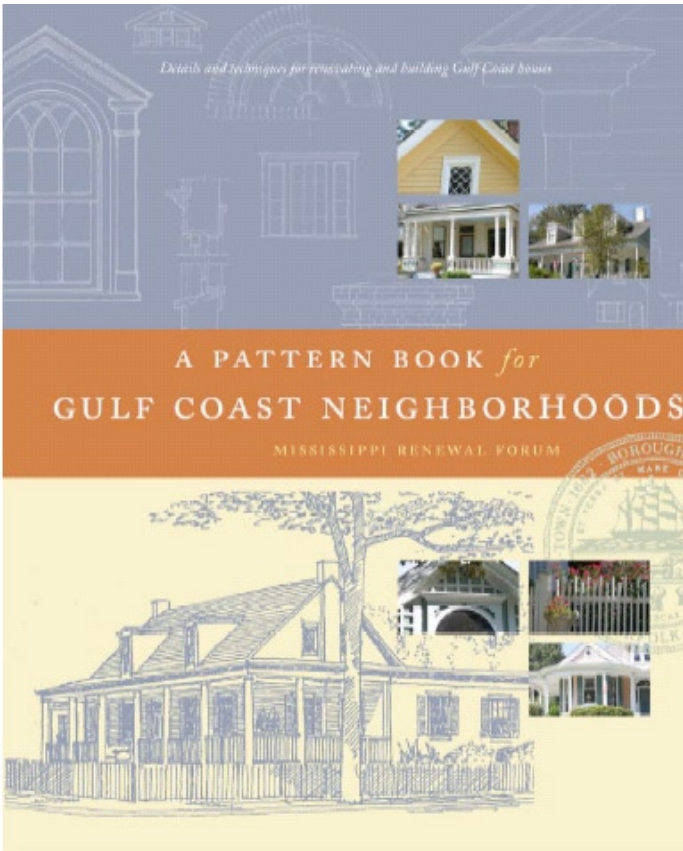
A.

Residential Building Types and Pattern Books:

A repertoire of building types and pattern books has been provided by other members of the Congress for the New Urbanism team including: townhouses, courtyard buildings, side-yard houses, patio houses, detached houses, apartment buildings, etc. A similar architectural syntax of its elements (windows, doors, balconies, roofs, etc.) and its proportions (including building heights) produces a certain degree of economy, propriety, and general efficiency.

In a similar fashion, public spaces (from the most urban to the most rural) have been identified. These public spaces include: front/

rear/side yards, courtyards, plazoletas, plazas, greens, parks, etc. More importantly, the Master Plan assumes a correlation between the open space / street type and the type of building frontage (within the same gradient – from urban to rural). Buildings which are more rural in character require larger front and lateral setbacks; more urban buildings assume smaller front setbacks and very small or null lateral setbacks.



B.



C.



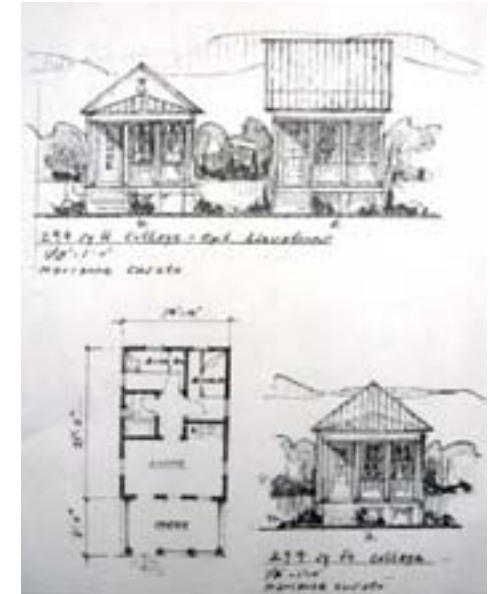
D.



E.

Colonnades and Porches:

In an attempt to restore the pedestrian friendly image of the downtown, the 2005 Citizens Master Plan calls for the reconstitution of public colonnades (arcades) along sections of South Central Avenue. The colonnades shall be incorporated into the new Land Development Regulations as allowed encroachments on the public right of way (ROW). These encroachments be no less than 10 feet deep and shall have a minimum frontage of 80%.



F.

D. *Street view of raised buildings.*

Drawing by Ben Pentreath.

E. *Street View.*

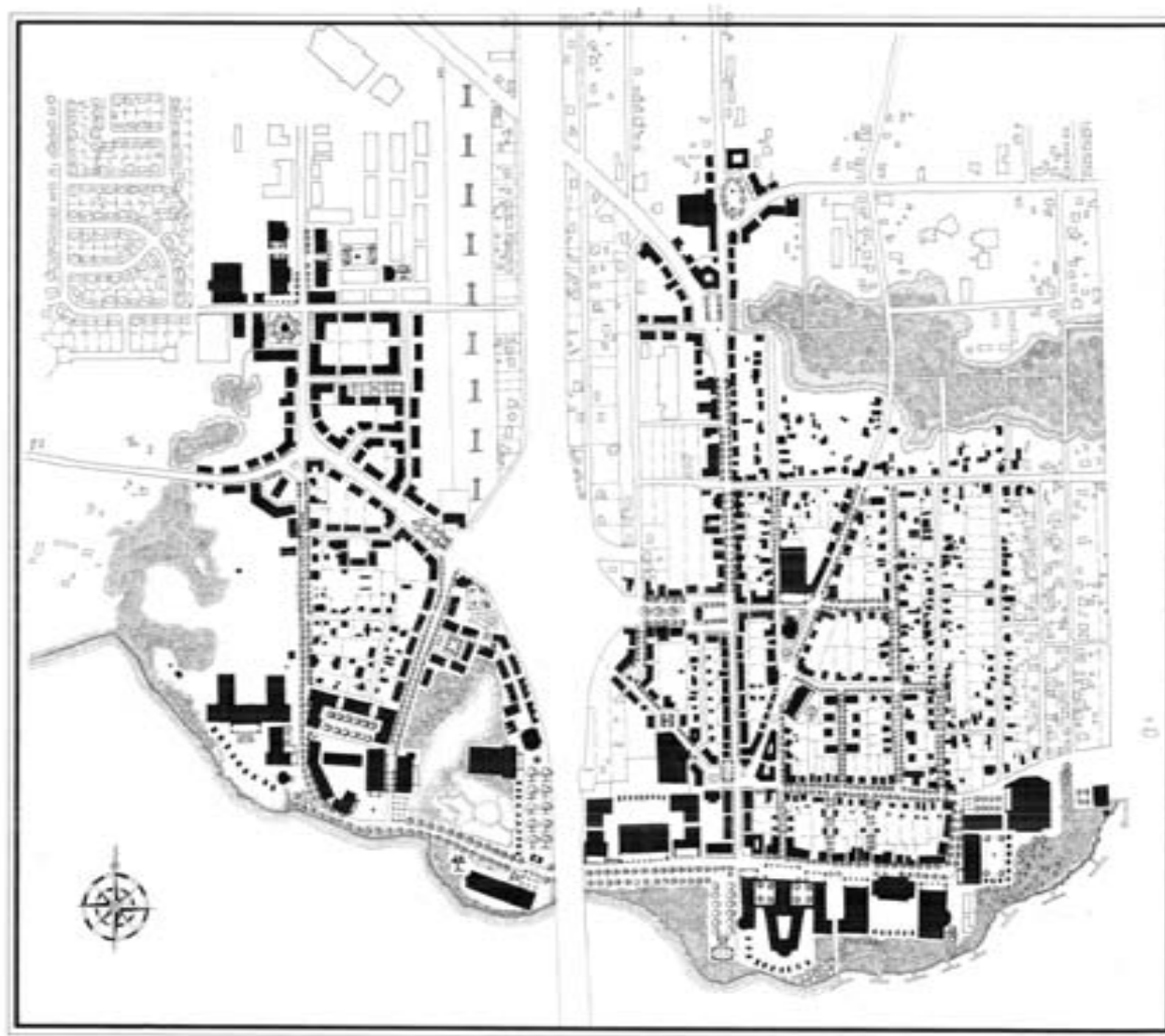
Drawing by David R. Csont, Urban Design Associates.

F. *Single family buildings.*

Drawings by CNU Architecture Team.

The disappearance of the porch as a semi-public space may have come from many sources, including the invention of residential air conditioning, but its consequences have been devastating for the social structure of our communities. The 2005 Citizens Master Plan calls for the re-introduction of porches into residential and live/work units. Porches must be incorporated into the new Land Development Regulations as allowed encroachments into the front and corner setbacks. These encroachments shall be no less than 8 ft. and shall occupy a minimum frontage of 50%.

Conclusion



A.

The public officials and citizens of D'Iberville have spoken clearly. The 2005 D'Iberville Citizens Master Plan is not another scream into the void; it is the most comprehensive summary of the natural, physical, spiritual, and human goals of a community hurt by an unexpected devastation.

The D'Iberville 2005 Citizens Master Plan is a testimony of the superb urban traditions established by its City Founder at the end of the seventeenth century.

Please do not be put off by the elegance and excellence of these drawings. The dichotomy wherein workable plans are mediocre in their presentation, while beautifully done plans are impossible to implement, is not present here. This Master Plan is both.

We certainly hope that this report is influential beyond this region, and that its working method becomes a paradigm when dealing with urban design emergencies in the rest of America.

B. View of open-air market.

Drawing by
Ditta Trisnawan for
Jaime Correa and
Associates.



B.

Appendix I: *Strategic Actions*

POLICY:

1. Amend Comprehensive Zoning Ordinance of 1995 (CZO'95) to allow arcades and boardwalk on proposed mixed-use French Quarter area between Rodriguez Street and Bay Shore Drive.
2. Amend CZO'95 to allow Live/Work units along Grand Avenue between Rodriguez Street and D'Iberville Boulevard.
3. Amend CZO'95 to reconfigure casinos into "campus-like" incremental buildings. Establish a maximum frontage for the various components of this new building type, particularly its casino, hotel, and cabana portions.
4. Amend Subdivision Regulations to pre-approve buildings and projects complying, at least 80%, with proposed master plan.
5. Regulate the use and location of the existing wetland network as a natural and environmental preservation area or as part of an amenity/educational package for the City of D'Iberville.
6. Amend Environmental Regulations to allow the creation of water retention, detention, and filtering areas in public spaces.
7. Amend Environmental Regulations to minimize site disturbances through Site Design.
8. Amend CZO'95 to prevent land consolidation and development speculation in single-family and rural areas.
9. Implement a public transit subsidy and study inter-local agreements with local merchants acting as public transportation subsidiaries.

MANAGEMENT:

1. Work on inter-local agreement to relocate the Harrison County Waste Management Plant located on south D'Iberville to a new site at the proposed City of D'Iberville Northern Annexation area.
2. Get a development commitment for a Seafood Park next to the new Waste Management Plant.
3. Acquire property at NW corner of Central Avenue and Race Track Road. This piece of property shall be used for an open-air market at the center of the proposed French Quarter entertainment district.
4. Acquire open site south of City Hall. This property shall be dedicated to the creation of a traditional Civic Square with a band stand.



DESIGN:

5. Acquire right-of-way and properties between Rae Street and D’Iberville Boulevard. This site will allow a non-disruptive connection between Popps Ferry Road and Lemoyne Boulevard and, as a consequence, the creation of a new North-South local connector road between Gulfport and Ocean Springs.

6. Acquire Don Ford, Inc. site (car dealership site) and develop a private/public partnership to develop a multi-family housing project with the highest new urbanism design standards.

7. Reconstruct the D’Iberville Middle-School and School Attendance Center to the open site on Big Ridge Road. Use the existing site for a small elementary school and new mixed-use development.
1. Design city entrances on Rodriguez Street at the exit of Interstate 110.

2. Connect 5th Avenue and Santa Cruz Avenue to provide a direct access to the proposed Casino Row on Bay Shore Drive

3. Create an esplanade along Bay Shore Drive.

4. Extend Bay Shore Drive to Fournier Avenue.

5. Establish an environmental network from City Hall to the location of the re-constructed Environmental and Educational Center at Antoine Fountain Cemetery.

6. Build new open-air market on Race Track Road and Central Avenue.
7. Plant trees along the main connector roads.

8. Build Middle-School campus on Big Ridge Road and a small Magnet Elementary School at the location of the old D’Iberville School Attendance Center.

10. Reconfigure the intersections of D’Iberville Boulevard, Central Avenue North, and Lemoyne Boulevard.

11. Enhance the transportation efficiency with the creation of a public transportation network between neighborhood centers.

12. Implement a small Jitney/Tram between French Quarter District and Walmart –along Central Avenue.
13. Establish a water-taxi program between the casinos on Bay Shore Drive and the City of Biloxi.

14. Open mid-block connections to enhance walkability and to diminish the size of blocks inherited from the French Land Grant programs of c.1655.

15. Clean up the existing Waste Management Plant brown field and re-locate to Northern Annexation area.

Appendix II: *Preliminary Codes*

The following eleven pages are a summary of the team's proposal for preliminary codes. The codes are site specific and applicable to paradigmatic sites within the 2005 D'Iberville Citizens Master Plan.

The code proposals, however, are not prepared with the intention of providing a high degree of technical proficiency. On the contrary, they must be understood as the bare minimum standards for the clear definition of true traditional public spaces and architecture. They are an insurance against the lack of predictability of suburbia and against the lack of understanding of traditional public spaces on the hands of lawyers, developers, architects, and construction personnel.

It is our hope that public officials, staff, and citizens use them as a template for discussion and preservation of the general vision presented in this report.

A lot more work needs to be done. This is only the beginning.

U R B A N S T A N D A R D S

SITE ONE: Civic Plaza

Lot Occupation

Lot Width: Varies. Missing lot lines shall be completed by Faculty.

Lot Coverage: 80% Maximum.

Building Setback

Front: 0 ft. or 12 ft.

Corner: 0 ft. or 12 ft.

Side: 0 ft. or 5 ft.

Rear: 3 ft. min.

Private Frontages

Porches: 10 ft. max. front or lateral encroachment.

Stoops: Permitted.

Arcades: 12 ft. max. R.O.W. encroachment.

Building Height and Building Function

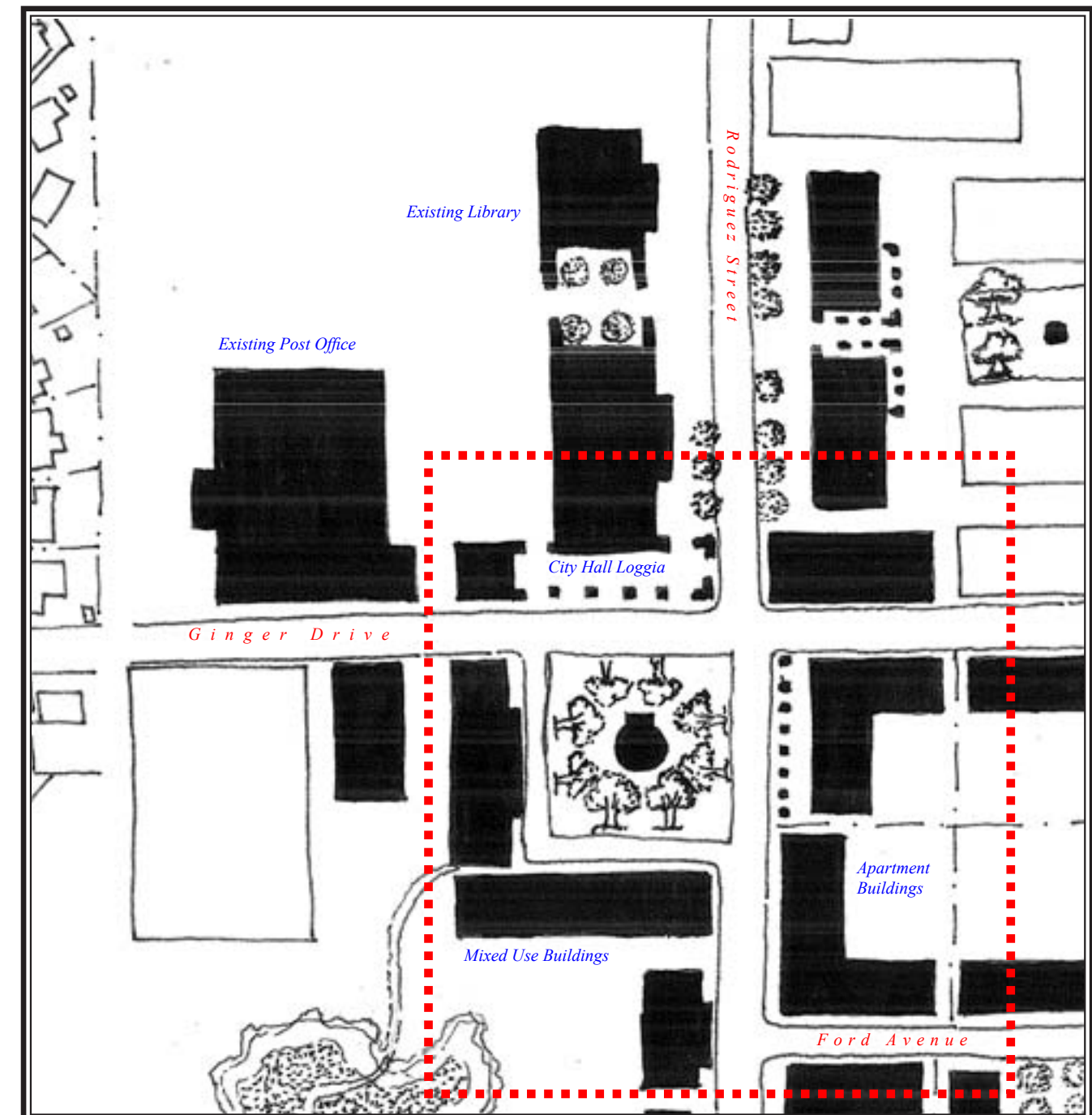
Principal Building: Max. 35 ft. over FEMA Base Flood Elevation (BFE)
Measured to cornice line or top of parapet

Outbuildings: Max. 25 ft. over FEMA Base Flood Elevation (BFE)
Measured to cornice line or top of parapet

BFE: Ground floor at 1 ft. min.

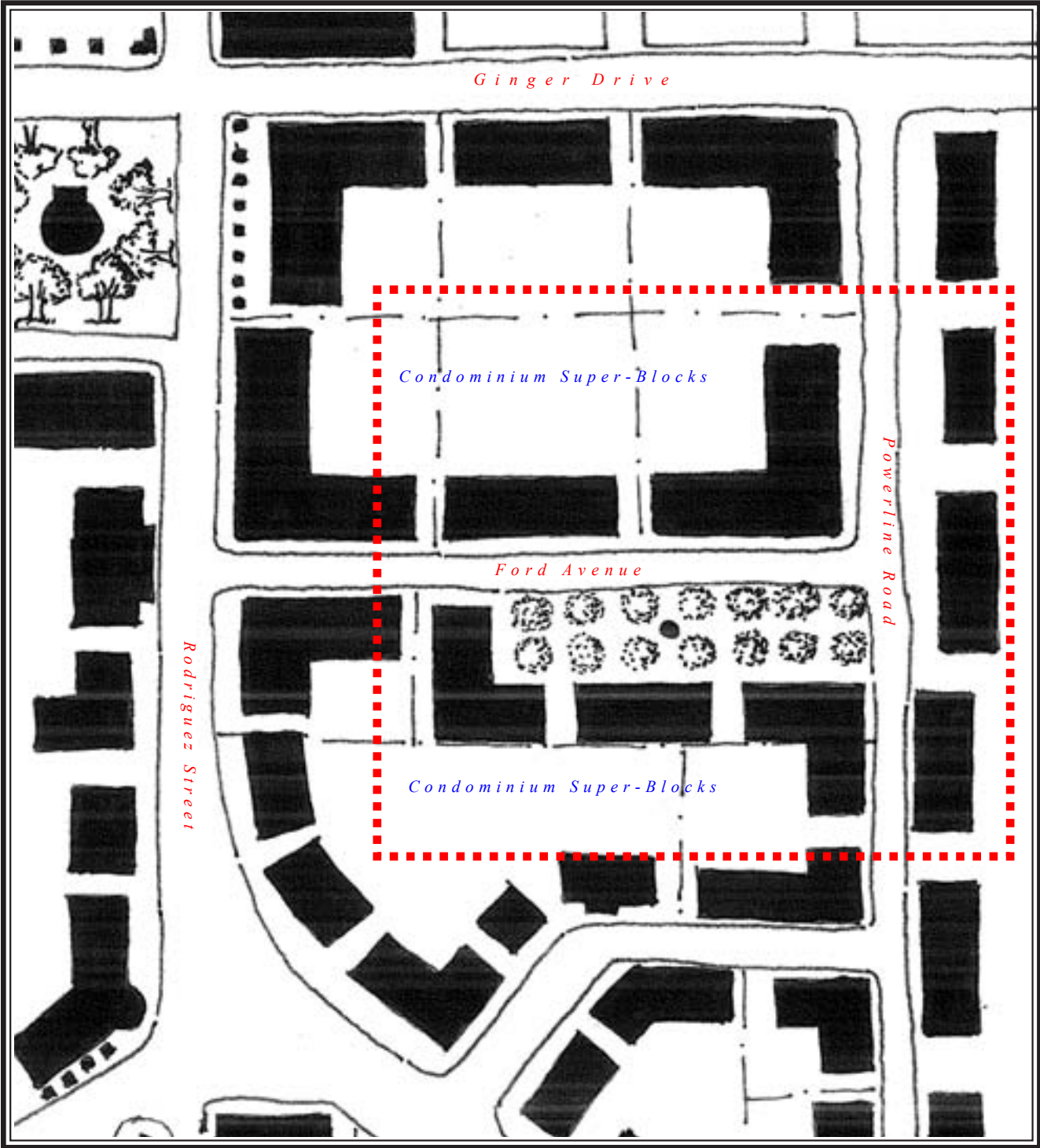
Building Function: Refer to attached site plan diagram.

0 ft. 100 ft. 200 ft. 300 ft.



SITE TWO: Residential Super-Blocks

U R B A N S T A N D A R D S



Lot Occupation

Lot Width:	Varies. Missing lot lines shall be completed by Faculty.
Lot Coverage:	70% Maximum.

Building Setback

Front:	Use frontage lines on attached site plan diagram.
Corner:	Use frontage lines on attached site plan diagram.
Side:	5 ft. min. 10 ft. max.
Building Depth:	64 ft. Max.

Private Frontages

Porches:	10 ft. max. front encroachment.
Stoops:	Permitted.
Terraces:	Permitted

Building Height and Building Function

Principal Building:	Max. 35 ft. over FEMA Base Flood Elevation (BFE) Measured to cornice line or top of parapet
BFE:	Ground floor at 1 ft. min.
Building Function:	Refer to attached site plan diagram.

U R B A N S T A N D A R D S

SITE THREE: Bayside Condominiums

Lot Occupation

- Lot Width: Varies. Missing lot lines shall be completed by Faculty.
- Lot Coverage: 70% Maximum.

Building Setback

- Front: Use frontage lines on attached site plan diagram.
- Corner: Use frontage lines on attached site plan diagram.
- Side: Use frontage lines on attached site plan diagram.
- Building Depth: 64 ft. Max.

Private Frontages

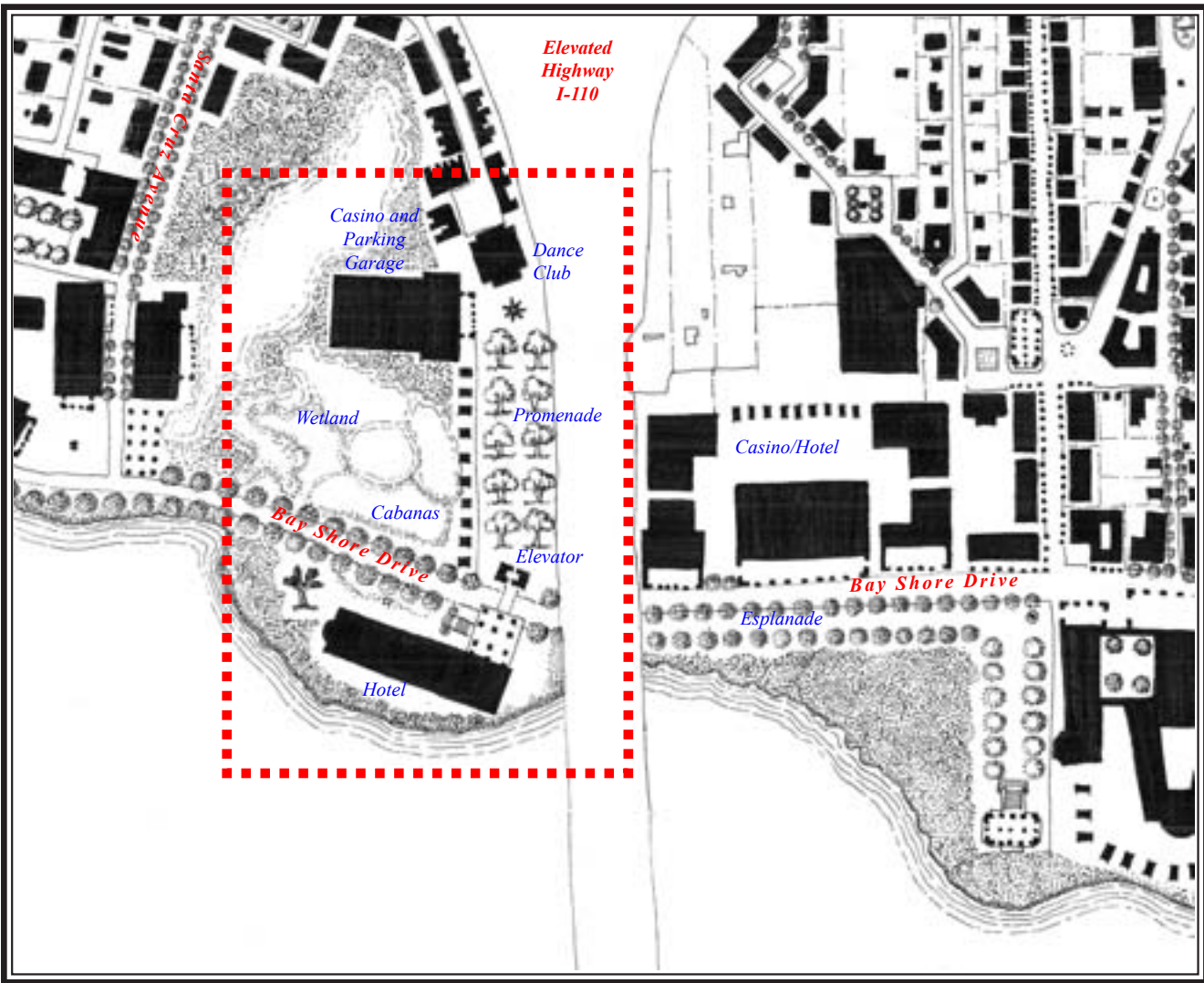
- Porches: Permitted (12 ft. Min. Suggested)
- Stoops: Permitted.
- Terraces: Permitted

Building Height and Building Function

- Principal Building: Main Building Max. 300 ft. over FEMA Flood Elevation (BFE)
Secondary Building Max. 65 ft. over BFE
Cabanas Max. 35 ft. over BFE
Measured to cornice line or top of parapet
- BFE: Ground floor at 24 ft. min. except open-air spaces
- Building Function: Refer to attached site plan diagram.



SITE FOUR: Hotel and Casino (Casino Row)



U R B A N S T A N D A R D S

Lot Occupation

Lot Width:	Varies. Missing lot lines shall be completed by Faculty.
Lot Coverage:	70% Maximum.

Building Setback

Front:	Use frontage lines and location map on attached site plan diagram.
Corner:	Use frontage lines and location map on attached site plan diagram.
Side:	Use frontage lines and location map on attached site plan diagram.
Building Depth:	64 ft. Max.

Private Frontages

Porches:	Permitted
Stoops:	Permitted.
Terraces:	Permitted

Building Height and Building Function

Principal Building:	Hotel Max. 300 ft. over FEMA Flood Elevation (BFE) Casino Max. 65 ft. over BFE Cabanas Max. 35 ft. over BFE Measured to cornice line or top of parapet
BFE:	Ground floor at 24 ft. min. except open-air spaces
Building Function:	Refer to attached site plan diagram.

U R B A N S T A N D A R D S

S I T E F I V E : French Quarter Market

Lot Occupation

Lot Width: Varies. Missing lot lines shall be completed by Faculty.

Lot Coverage: 80% Maximum.

Building Setback

Front: Ground Floor 10 ft. Required
Second Floor and Up 0 ft. Required

Corner: Ground Floor 10 ft. Required
Second Floor and Up 0 ft. Required

Side: Ground Floor 10 ft. Required
Second Floor and Up 0 ft. Required

Rear: Principal Building 40 ft. Min.; Outbuilding 3 ft. Min.

Private Frontages

Porches: 10 ft. max. Front or Corner Encroachment (raised 4 ft. over BFE)

Handicap Ramps: Not Visible from R.O.W. (preferably internalized)

Arcades: 10 ft. Max. R.O.W. Encroachment (same width as sidewalk)

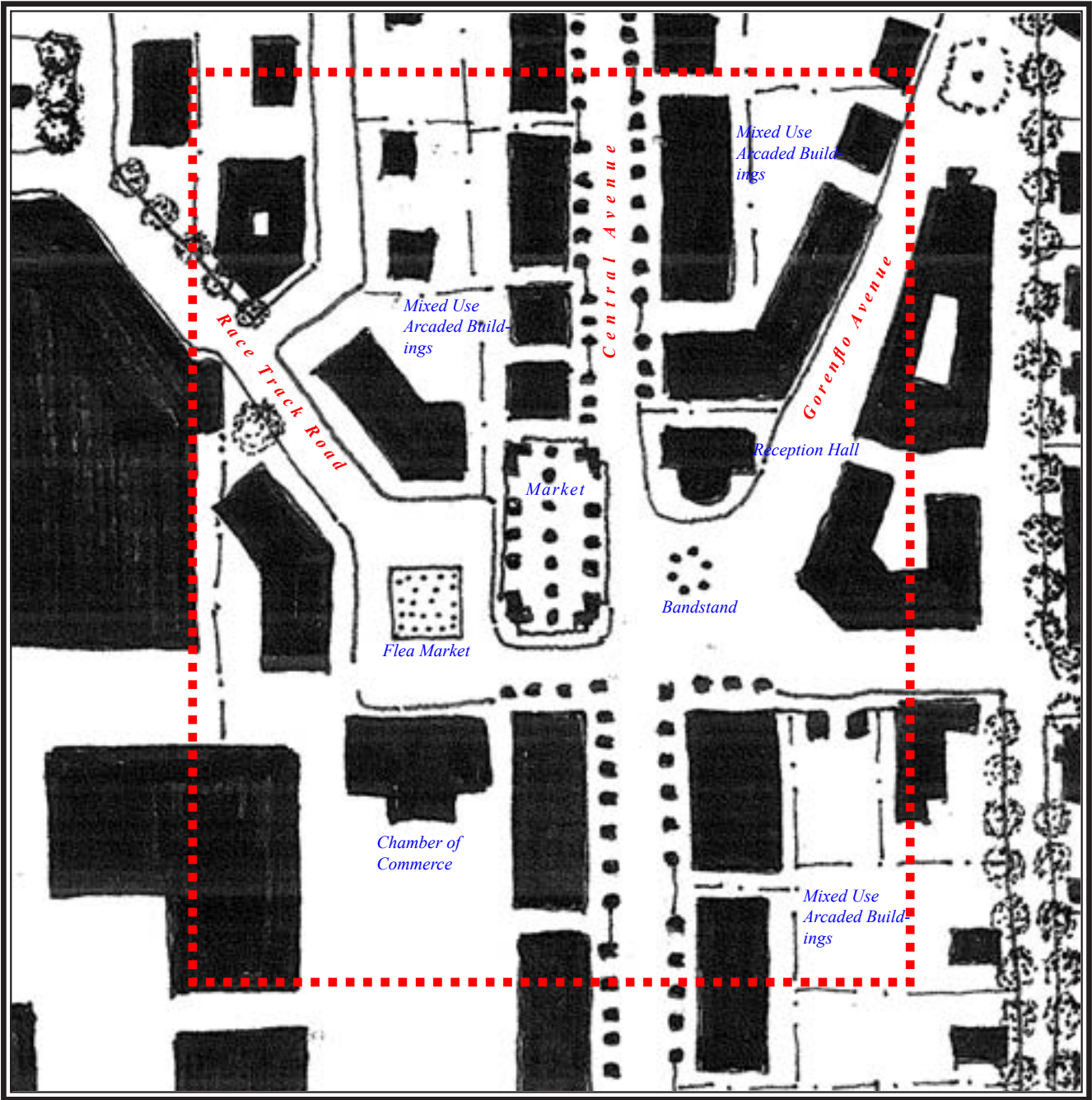
Building Height and Building Function

Principal Building: Max. 35 ft. over FEMA Base Flood Elevation (BFE)
Measured to cornice line or top of parapet
Civic/Public Buildings No Height Limitation

Outbuildings: Max. 25 ft. over FEMA Base Flood Elevation (BFE)
Measured to cornice line or top of parapet

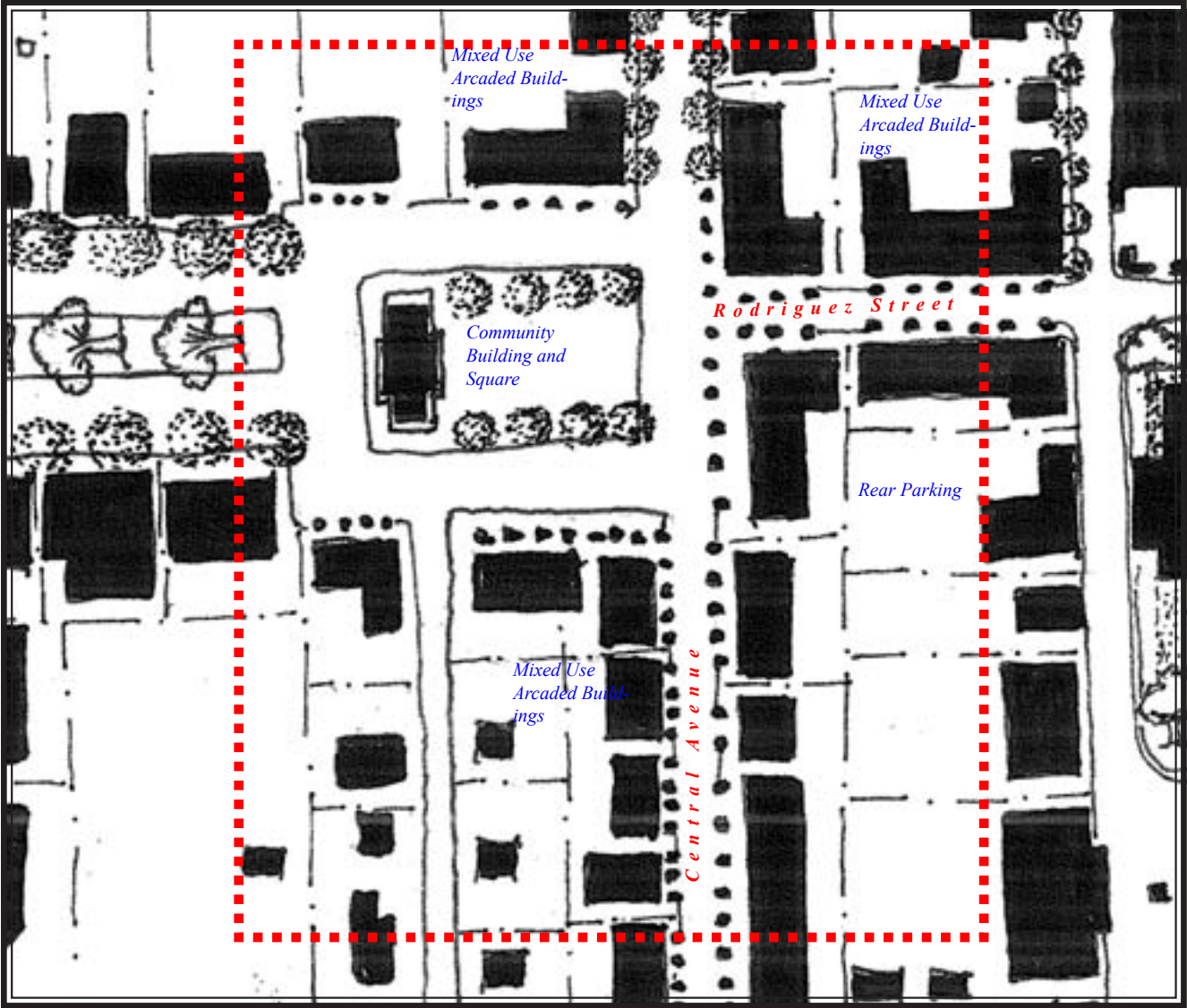
BFE: Ground floor at 4 ft. min.

Building Function: Refer to attached site plan diagram.



SITE SIX: French Quarter Entrance Square

U R B A N S T A N D A R D S



Lot Occupation

- Lot Width:** Varies. Missing lot lines shall be completed by Faculty.
- Lot Coverage:** 80% Maximum.

Building Setback

- Front:** Ground Floor 10 ft. Required
Second Floor and Up 0 ft. Required
- Corner:** Ground Floor 10 ft. Required
Second Floor and Up 0 ft. Required
- Side:** Ground Floor 10 ft. Required
Second Floor and Up 0 ft. Required
- Rear:** Principal Building 40 ft. Min.; Outbuilding 3 ft. Min.

Private Frontages

- Porches:** 10 ft. max. Front or Corner Encroachment (4 ft. over BFE)
- Handicap Ramps:** Not Visible from R.O.W. (preferably internalized)
- Arcades:** 10 ft. Max. R.O.W. Encroachment (same width as sidewalk)

Building Height and Building Function

- Principal Building:** Max. 35 ft. over FEMA Flood Elevation (BFE)
Measured to cornice line or top of parapet
Civic/Public Buildings No Height Limitation
- Outbuildings:** Max. 25 ft. over FEMA Flood Elevation (BFE)
Measured to cornice line or top of parapet
- BFE:** Ground floor at 4 ft. min.
- Building Function:** Refer to attached site plan diagram.

U R B A N S T A N D A R D S

Lot Occupation

Lot Width: Varies. Missing lot lines shall be completed by Faculty.

Lot Coverage: 60% Maximum.

Building Setback

Front: 0 ft. or 14 ft.

Corner: 0 ft. or 14 ft.

Side: 0 ft. or 5 ft.

Rear: Principal Building 40 ft. Min.; Outbuilding 3 ft. Min.

Private Frontages

Porches: 10 ft. max. Front or Corner Encroachment (2 ft. Min. over BFE)

Handicap Ramps: Not Visible from R.O.W. (preferably internalized)

Terraces: Permitted

Building Height and Building Function

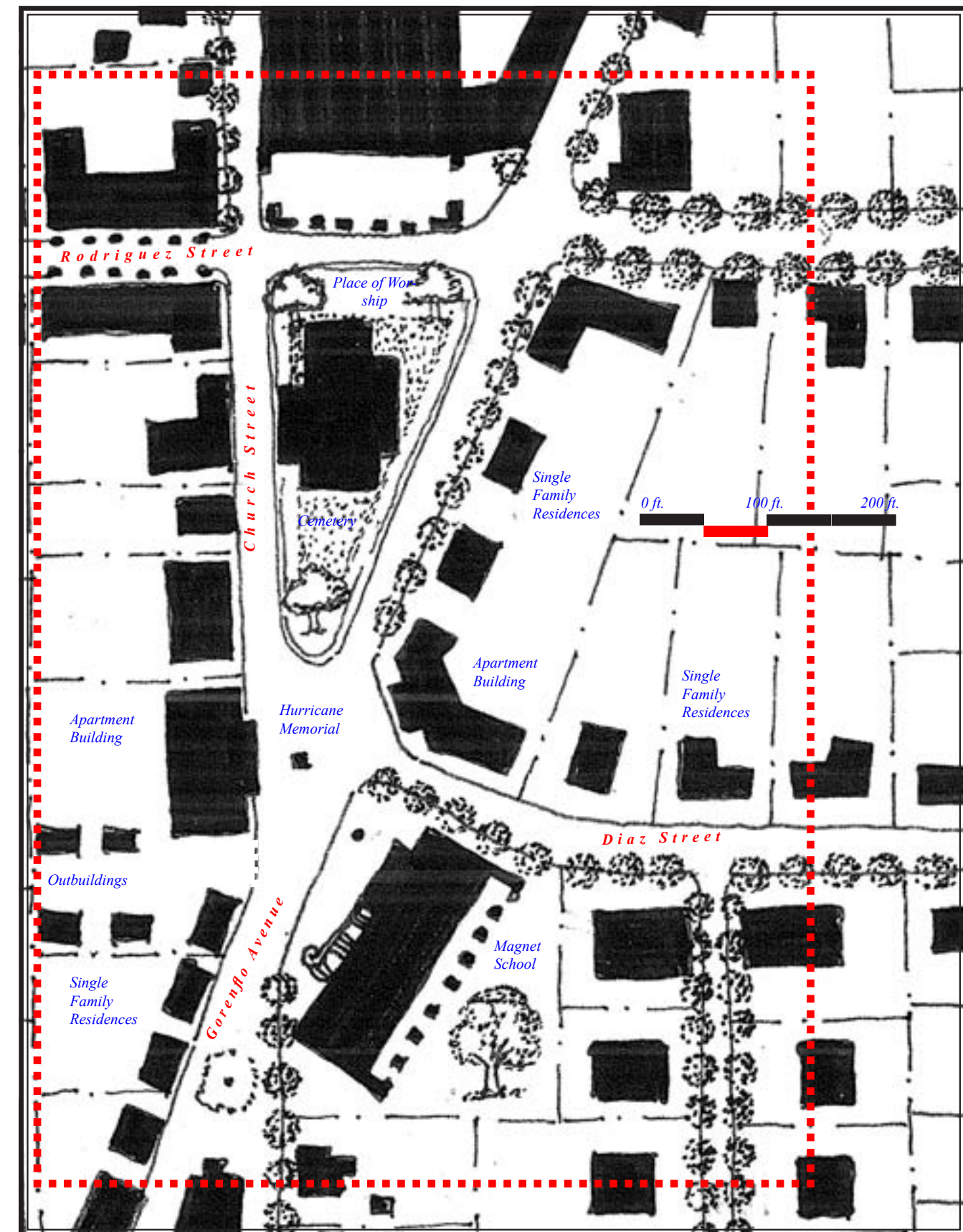
Principal Building: Max. 35 ft. over FEMA Base Flood Elevation (BFE)
Measured to cornice line or top of parapet
Civic/Public Buildings No Height Limitations

Outbuildings: Max. 25 ft. over FEMA Base Flood Elevation (BFE)
Measured to cornice line or top of parapet

BFE: Ground floor at 4 ft. min.

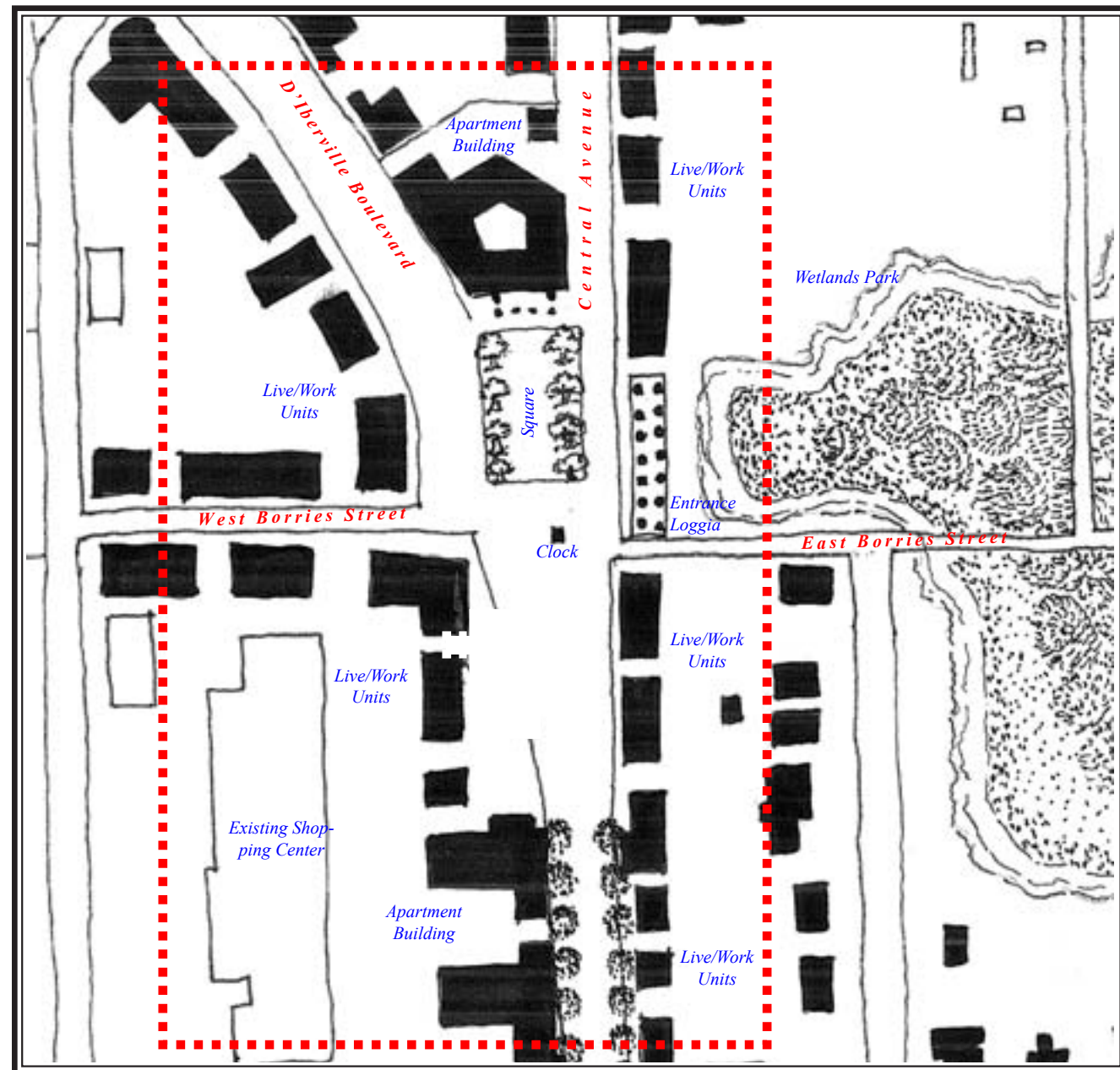
Building Function: Refer to attached site plan diagram.

SITE SEVEN: Magnet School and Cemetery



SITE EIGHT: Live/Work Square and Park Entrance

0 ft. 100 ft. 200 ft.



U R B A N S T A N D A R D S

Lot Occupation

Lot Width: Varies. Missing lot lines shall be completed by Faculty.

Lot Coverage: 60% Maximum.

Building Setback

Front: See Frontage Lines in Attached Site Plan Diagram
Buildings with Porches Must Be Setback 14 ft. Additional

Corner: See Frontage Lines in Attached Site Plan Diagram
Buildings with Porches Must Be Setback 14 ft. Additional

Side: 0 ft. or 5 ft.

Rear: Principal Building 40 ft. Min.; Outbuilding 3 ft. Min.

Private Frontages

Porches: 10 ft. max. Front or Corner Encroachment

Handicap Ramps: Not Visible from R.O.W. (preferably internalized)

Terraces: Permitted

Arcades: Permitted (12 ft. Max. Encroachment)

Building Height and Building Function

Principal Building: Max. 35 ft. over FEMA Base Flood Elevation (BFE)
Measured to cornice line or top of parapet
Civic/Public Buildings No Height Limitations

Outbuildings: Max. 25 ft. over FEMA Base Flood Elevation (BFE)
Measured to cornice line or top of parapet

BFE: Ground floor at 2 ft. min.

Building Function: Refer to attached site plan diagram.

U R B A N S T A N D A R D S

SITE NINE: Mixed Use Rotonda

Lot Occupation

Lot Width: Varies. Missing lot lines shall be completed by Faculty.

Lot Coverage: 60% Maximum.

Building Setback

Front: See Frontage Lines in Attached Site Plan Diagram
Buildings with Porches Must Be Setback 14 ft. Additional

Corner: See Frontage Lines in Attached Site Plan Diagram
Buildings with Porches Must Be Setback 14 ft. Additional

Side: 0 ft. or 5 ft.

Rear: Principal Building 40 ft. Min.; Outbuilding 3 ft. Min.

Private Frontages

Porches: 10 ft. max. Front or Corner Encroachment

Handicap Ramps: Not Visible from R.O.W. (preferably internalized)

Terraces: Permitted

Arcades: Permitted (12 ft. Max. Encroachment)

Building Height and Building Function

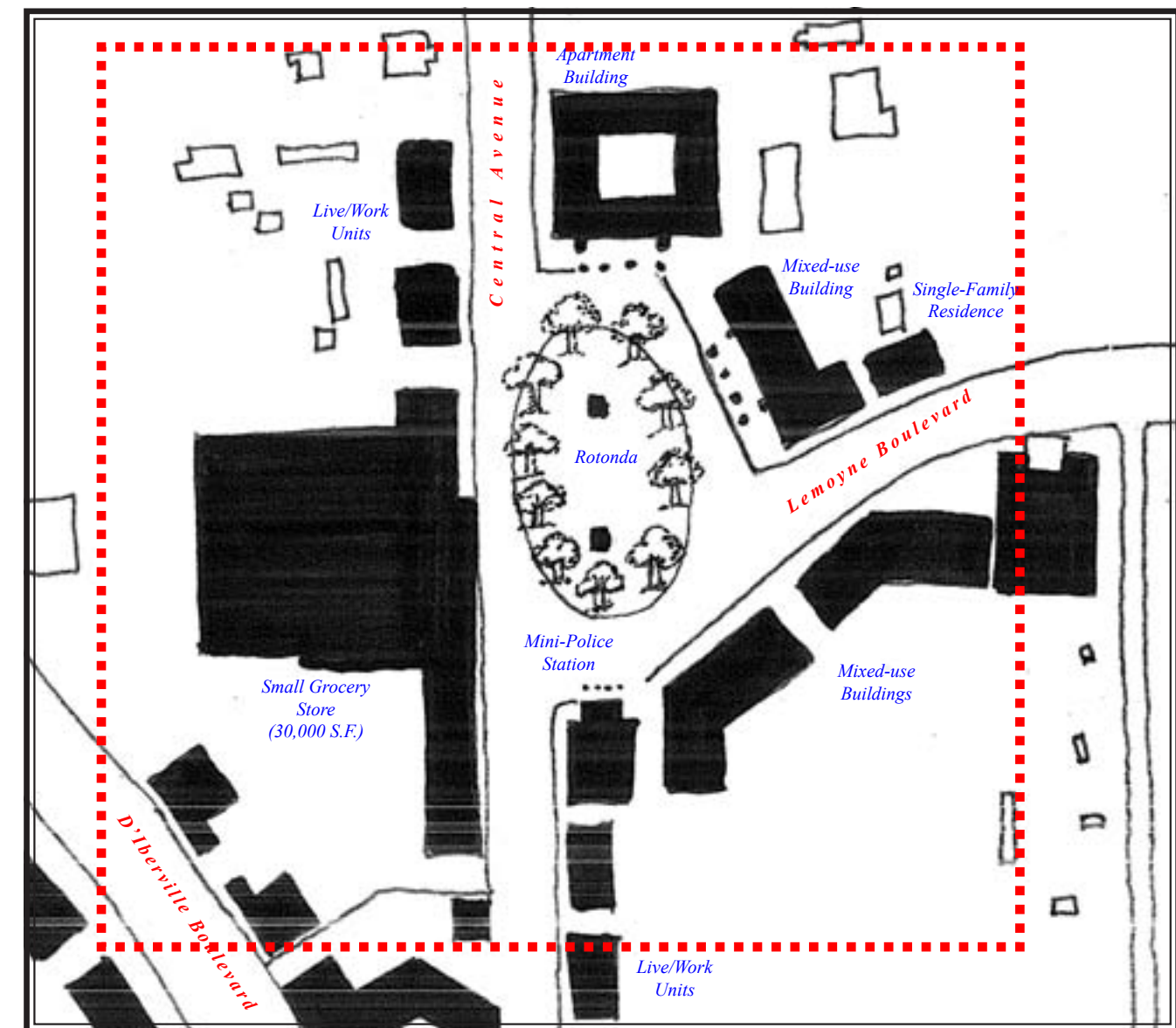
Principal Building: Max. 35 ft. over FEMA Base Flood Elevation (BFE)
Measured to cornice line or top of parapet
Civic/Public Buildings No Height Limitations

Outbuildings: Max. 25 ft. over FEMA Base Flood Elevation (BFE)
Measured to cornice line or top of parapet

BFE: Ground floor at 2 ft. min.

Building Function: Refer to attached site plan diagram.

0 ft. 100 ft. 200 ft.



SITE TEN: Detached Houses

0 ft. 100 ft. 200 ft.



U R B A N S T A N D A R D S

Lot Occupation

Lot Width: Varies. Missing lot lines shall be completed by Faculty.

Lot Coverage: 50% Maximum.

Building Setback

Front: 24 ft. Required

Corner: 24 ft. Required

Side: 5 ft. Min.

Rear: Principal Building 40 ft. Min.; Outbuilding 3 ft. Min.

Private Frontages

Porches: 12 ft. Req. Front or Corner Encroachment (4 ft. Min. over BFE)

Handicap Ramps: Not Visible from R.O.W. (preferably internalized)

Terraces: Permitted

Building Height and Building Function

Principal Building: Max. 35 ft. over FEMA Base Flood Elevation (BFE)
Measured to cornice line or top of parapet
Civic/Public Buildings No Height Limitations

Outbuildings: Max. 25 ft. over FEMA Base Flood Elevation (BFE)
Measured to cornice line or top of parapet

BFE: Ground floor at 8 ft. Min.

Building Function: Refer to attached site plan diagram.

U R B A N S T A N D A R D S

SITE ELEVEN: Neighborhood Center

Lot Occupation

- Lot Width:** Varies. Missing lot lines shall be completed by Faculty.
- Lot Coverage:** 60% Maximum.

Building Setback

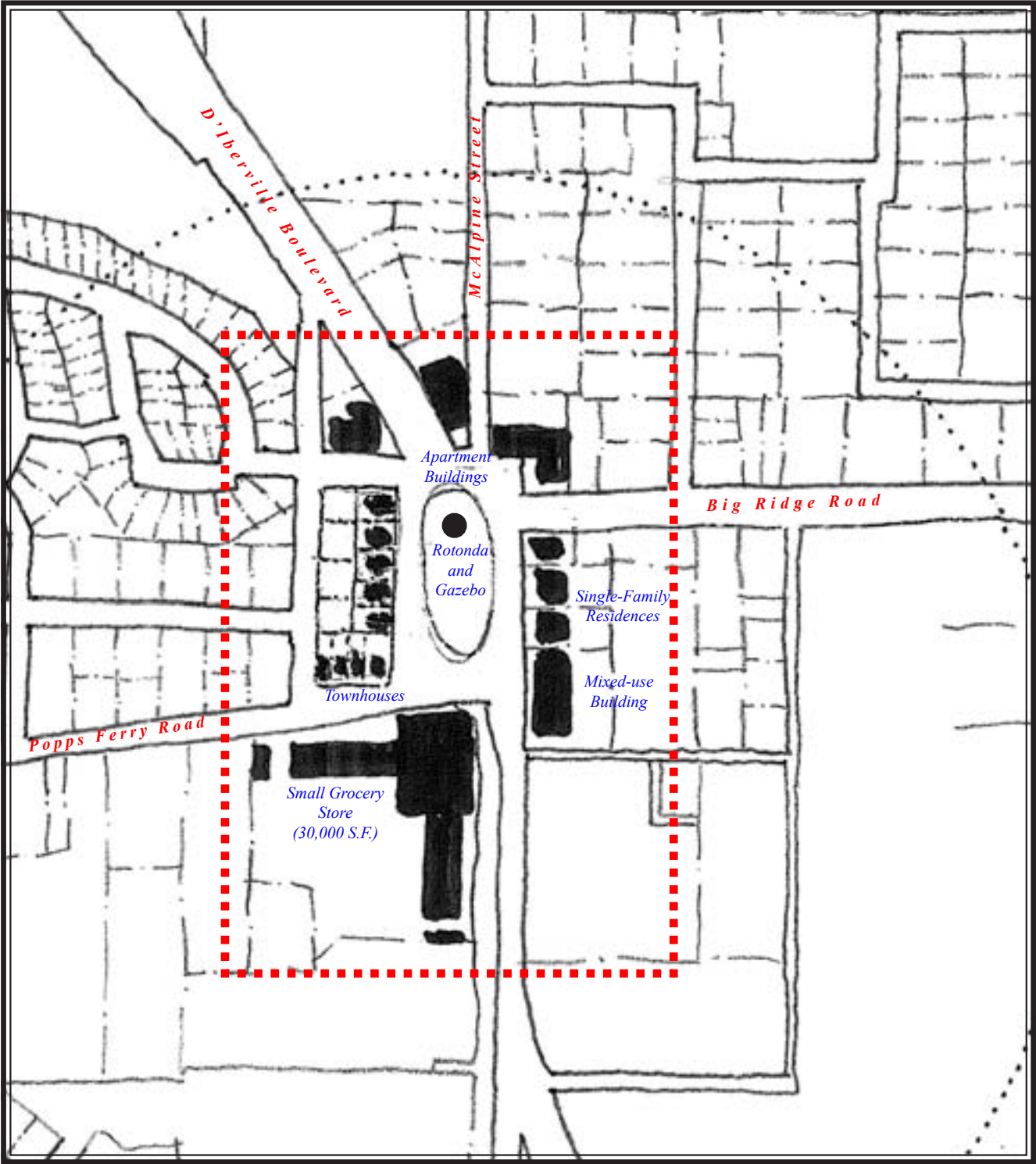
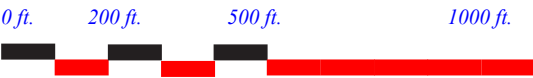
- Front:** 0 ft. or 10 ft.
Buildings with PorchesMust Be Setback 10 ft. Additional
- Corner:** 0 ft. or 10 ft.
Buildings with PorchesMust Be Setback 10 ft. Additional
- Side:** 0 ft. or 5 ft.
- Rear:** Principal Building 30 ft. Min.; Outbuilding 3 ft. Min.

Private Frontages

- Porches:** 10 ft. max. Front or Corner Encroachment
- Handicap Ramps:** Not Visible from R.O.W. (preferably internalized)
- Terraces:** Permitted

Building Height and Building Function

- Principal Building:** Max. 35 ft. over FEMA Base Flood Elevation (BFE)
Measured to cornice line or top of parapet
Civic/Public Buildings No Height Limitations
- Outbuildings:** Max. 25 ft. over FEMA Base Flood Elevation (BFE)
Measured to cornice line or top of parapet
- BFE:** Ground floor at 2 ft. min.
- Building Function:** Refer to attached site plan diagram.



A c k n o w l e d g e m e n t s

The Design Team would like to express its sincere appreciation for the opportunity given to us in the current state of emergency. This work would have been impossible without the organizational skills, knowledge, and undeniable leadership of Andres Duanny and Elizabeth Plater-Zyberk in collaboration with the Congress for the New Urbanism.



The Congress for the New Urbanism and the Mississippi Governor’s Commission would like to thank the following public officials for their kind participation:

- Rusty Quave - Mayor
- Richard Rose - City Manager
- Brenda Broussard - Ward 1
- Henry Toncrey - Ward 2
- Teddy Harder - Ward 3
- Bob Bellman - Ward 4
- Glenn Ellis - At Large
- Deonne Olier - Planning and Zoning
- Wallace Freeman - Building Codes
- Mike Mullins - Parks and Recreation
- Mark Seymour - Engineering
- Bubby Eleuterius - County Supervisor

The following personnel formed part of the Design Team:

- Jaime Correa
- Grace Dillon
- Manuel De Lemos
- Francisco Llado
- Ditta Trisnawan

The final report was produced in the studios of Jaime Correa and Associates in Miami, Florida.

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CONGRESS FOR THE NEW URBANISM

